

Development Review Team Memo

(Applicant Response in red – Dated 8/5/2025)

FROM: Kelly Douglas, AICP – Senior Planner

DATE: 2025-04-08

RE: PL20250063 Basecamp Phase II 1850 Luna Ln
Development Plan, Conditional Use and Major Variance
Planning Review Submittal #1

Big Picture Feedback

The current proposal relies on a development-wide approach to both parking and snow storage in order to function. Because the viability of the project depends on this integrated approach, what I'm referring to as the "development-wide application thread" must be followed through to the site design, with particular attention to how it addresses key public frontages.

Per 437.C.1.a, development is required to define the edges of and orient access to primary public and private streets, pedestrian circulation, and gathering areas. The Elk River Rd / Lincoln Ave intersection is one of the most significant intersections west of downtown, yet the proposal orients the amenity space—not the building—toward this important corner, with no clear access or frontage emphasis.

This site was previously approved with a commercial component at the Elk River/Lincoln corner—an opportunity that would be precluded by the current layout. While this is not necessarily to suggest that staff is unsupportive of the proposed Multiple-Family and Workforce Unit uses, it's important to note that the current configuration doesn't place either a commercial or residential use at this major intersection. As a result, the corner appears deactivated—an outcome that seems misaligned with the CS zone district purpose and the Future Land Use Map Commercial Activity Node designation, both of which call for a higher intensity of development. **Revised Development Plan proposes a commercial market/restaurant use on the Elk River/US40 corner to help address the above concerns. The architectural design has not been developed yet for this concept, however the proposed size, use(s), and parking are shown with a building footprint (final building footprint subject to change with a full DP submittal at a future date, however the use and max. size will not).**

In addition, the proposal requires a Conditional Use for the surface parking lot on Lot 3. Approval criterion 707.C.3 requires that negative impacts be mitigated. Multiple-Family Residential Design Standards purpose statement 437.A.3 helps identify one such impact, directing that parking areas should not become the dominant use of common space in multiple-family developments. The current design, which places surface parking at the Elk River Rd. frontage, may not align with this guidance. **The revised design still proposes surface parking at a portion of the Elk River frontage, however it also proposes a commercial building at this location as the primary use on this frontage, with the parking primarily located to support the commercial use. Conditional use for the surface parking lot on Lot 3 has been added to the project narrative.**

In summary, if the project requires a site-wide solution to parking and snow storage, it should also demonstrate a site-wide approach to design—particularly under the lens of the standards in Section 437. Staff encourages the applicant to re-evaluate the site plan to:

- Reflect a stronger building relationship with Elk River Rd and Lincoln Ave—perhaps by swapping the building and open space/parking locations and including a ground-floor commercial use;

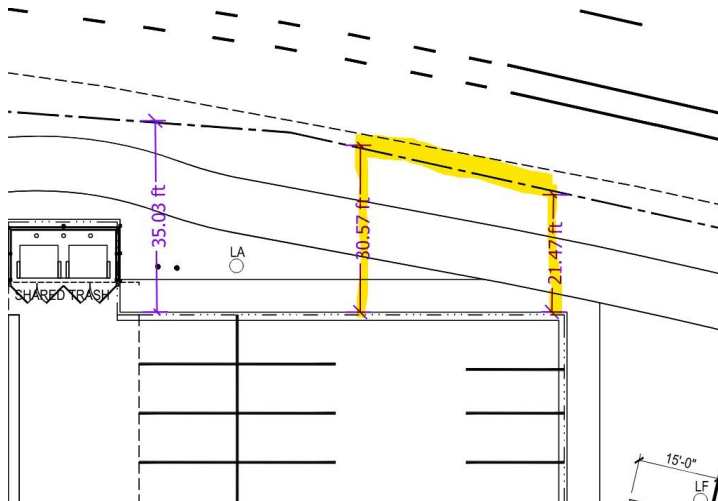
- Enhance and preserve long-term activation and vitality at this key intersection; and
- Ensure the overall site layout supports a functional, integrated, and policy-consistent development pattern.

General

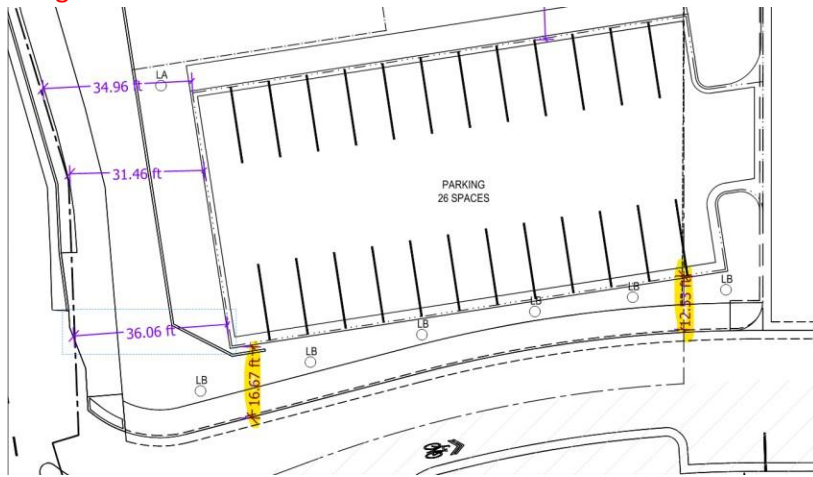
1. Please revise the coversheet table to ensure all applicable dimensional standards are accurately addressed, including but not limited to frontage height. **Coversheet table has been revised to include all applicable dimensional standards.** Confirm consistency with the CS zoning district requirements. For example, the open space percentage is currently listed as “???” and should be clarified. **Open space percentage has been clarified on the Civil Plans, see L.003.**
2. While the clean layout of the architectural site plan is appreciated, please include and clearly label property lines as well as existing and proposed easements. These may be shown on a separate sheet if necessary for clarity. **Property lines and easements have been added to the site plan. Existing easements have been included on a supplemental sheet A1.2 so as not to clutter the plan too much.**

Section 224 Zone District: Commercial Services (CS)

3. The location of the property line and access easement along Big Bend Dr is necessary to verify that the proposed building complies with the maximum front setback requirement in the CS zone. Please label these on the site plan or provide a separate exhibit. **Property line and access easement along Big Bend Dr. have been added to the site plan.**
4. The lot area shown on the coversheet only reflects Lot 2. Each lot must independently meet the minimum lot width, depth and area requirements of the CS zone. Please confirm and clearly show that both Lot 2 and Lot 3 meet these standards. **Lot 3 area and information has been added to the coversheet. Both lots meet the minimum lot width, depth and area requirements.**
5. It appears that a portion of the Lot 3 parking lot encroaches into the required 30' front setback for parking areas. Please revise the site layout to ensure compliance with the CS zone parking lot placement standards. **Site plan layout has been revised to ensure compliance with parking lot placement standards. New tandem parking spaces have been created as a result. These will be reserved for 2 bedroom units.**



6. It appears a portion of the Lot 2 parking lot encroaches into the required 30' front setback for parking lot placement. Per Section 801.B.1.a.ii, this setback must be measured from the private street easement. Please revise the plan to ensure compliance. **Parking lot has been shifted plan north approx. 8', i.e. as much as possible while still maintaining a viable commercial building footprint area, with adequate pedestrian circulation in front of it, on the subject lot plan north of the parking area. A variance has been included in the Project narrative, as it still does not meet standards with the new design.**



7. Please add and label existing grade to all elevations in order to confirm compliance with overall height and average plate height standards. **Existing grades have been added to each elevation, represented by a dashed line.**
8. Please label the top of the uppermost wall plate of the exterior wall that bears the building's highest roof structure on all elevations in order to verify frontage height. **Wall plate elevations have been labeled on the elevations.**
9. Lot coverage and floor area ratio (FAR) must be calculated separately for each lot. While both appear to meet the applicable standards, staff is getting different values than those shown on the coversheet. Please double-check the calculations for each lot and revise or clarify as needed. **Lot coverage and FAR values have been verified and updated for both Lot 2 and Lot 3 on coversheet.**

Section 237 Airport Overlay

10. Draft Condition of Approval: Prior to Building Permit issuance, the property owner shall record an aviation easement. **Noted**
11. Draft Condition of Approval: Prior to Building Permit issuance, the property owner shall record an airport proximity disclosure. **Noted**

Article 3 Use Definitions and Standards

12. Workforce units are classified as a Limited Use in the CS zone. The current approach does not appear to fully address the pedestrian-active building frontage requirement. If the intent is to retain flexibility for any residential use at the ground level, the application should include a request to modify the pedestrian-active frontage use standard 301.B.5.b, and include this modification in the Conditional Use request. **The request to modify the pedestrian-active frontage has been included in the Conditional Use narrative.**
13. Draft Condition of Approval: The applicant shall enter into an agreement, acceptable to the City,

restricting the occupancy of the Workforce Units to qualified residents as defined by the Community Development Code. The agreement shall be recorded at the Routt County Clerk and Recorder prior to Certificate of Occupancy/Completion. **Noted**

14. Draft Condition of Approval: The owner or rental agency for the property shall provide an annual report to the Planning Director outlining compliance with the deed restricted workforce units. The report shall, at a minimum, list all workforce units and how the occupants for each unit met the definition of Qualified Resident as defined in Community Development Code Section 802. **Noted**
15. The proposed parking lot on Lot 3 is a Surface Lot per 302.J. This use is a Conditional Use in the CS zone. Please update the narrative to include this use as part of the Conditional Use request. Reasonable conditions, such as easements, or plat notes limiting changes in ownership or use, may be appropriate to ensure long-term consistency with the approval. Please also see the Big Picture Feedback section. **Noted, this conditional use request been added to the narrative.**
16. The plans show a proposed dog park. Please clarify whether this is intended as a private amenity for residents and tenants, or if it will operate as a public/commercial use open to non-residents. If the latter, additional review may be required to determine the appropriate use classification and zoning procedure. **Dog park has been removed from the DP and replaced with a commercial building that will be submitted for full DP approval at a later date; the uses and sizes have been identified on the site plan.**
17. The narrative states that 230 parking spaces are proposed for the overall development; however, staff counts 242 spaces based on the Parking Management Plan. Please reconcile this discrepancy. Note that any spaces reserved for loading—regardless of duration—should not be included in the total parking count. **After some revisions to the site plan, the total space count is now 233, however two spaces are reserved for loading/deliveries, and thus removed from the total available space count, making the total 231. Parking Management Plan has been updated to reflect this.**

Section 402 Landscaping

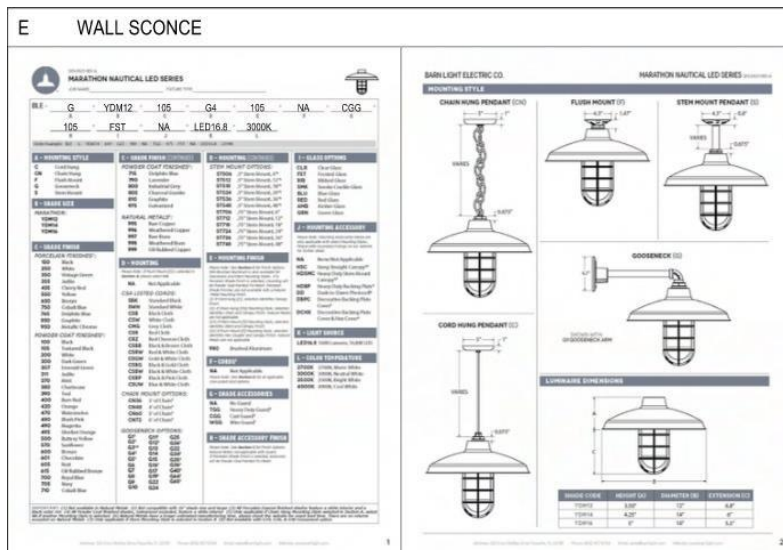
18. The landscape plan incorrectly states that Parking Lot Setback Landscaping is “N/A,” being covered by Frontage Landscaping. Per Section 402.D.2.c and Figure 402-2, when frontage landscaping areas and parking lot setback landscaping areas overlap, the parking lot setback landscaping standards apply. Please revise the plans accordingly to demonstrate compliance with these minimum standards. **Plans have been revised to include parking lot setback areas where they overlap with frontage landscaping.**
19. It does not appear Parking Lot Setback Landscaping has been applied. **Parking lot setback standards have been applied.**
20. Interior parking lot landscaping is shown in some areas where parking lot setback landscaping is required instead. These areas need attention to ensure the correct standards are being applied in the appropriate locations. Additionally, please confirm that both parking lots meet the minimum interior landscaping area square footage requirements per Table 402-4. **This issue has been resolved and parking lot setback standards have been applied.**



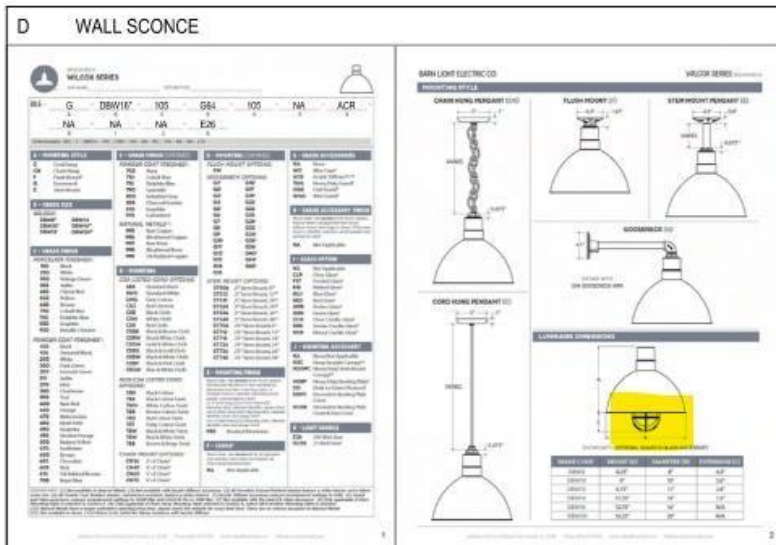
21. Staff has provided a “Section 402 Landscape Worksheet” and “PS25-0010-Landscape Plans Submittal 1 KZD Mark Up” as reference documents to help clarify how the landscape plan was reviewed. **Noted.**

Section 405 Exterior Lighting

22. The Fixture E Wall Sconce light source is not fully shielded. **Updated wall sconce is now included with full shield.**



23. Fixture D Wall Sconce seems to have an option of light source that is not fully shielded. **Updated wall sconce is now included with full shield.**



Section 406 Off-Street Parking

24. The current proposal represents a development-wide parking approach, with parking for multiple uses distributed across multiple lots rather than provided on the same site as each use. Per Section 406.E.2 – Off-Site Parking Facilities, this arrangement may be considered through the Development Plan–Public Hearing process (which you are in) and approved by City Council. To support this request, please address all requirements of Section 406.E.2, including:

- Proximity of off-site facilities (within 600 feet);
- Demonstration that off-site parking will fulfill the intent and function of required parking;
- Evidence that the facilities are as usable and convenient as on-site spaces;
- Confirmation that the arrangement will not create traffic or visual impacts; and
 - See related comment in Big Picture Feedback regarding long-term implications of this arrangement; and
- Legal documentation (e.g., easements, plat notes) to ensure long-term availability and maintenance.
- Please also update the narrative accordingly to clearly identify this as a request for alternative compliance under 406.E.2

Narrative has been updated to include this request and address the requirements of Section 406.E.2.

25. Please identify the location and quantity of bicycle parking proposed for the development. This information should be clearly shown on the site plan and/or landscape plan and should demonstrate compliance with Table 406-4. **Bicycle parking has been added to the site plan, adjacent to the building entrance.**

Section 408 Refuse Management

26. Please label recycling in order to confirm compliance with [Ordinance No. 2899](#). **Recycling has been labeled on the site plan**

Section 409 Snow Storage

27. Please clarify which snow storage areas are existing and which are proposed with this phase of development. Snow storage areas should be graphically differentiated by phase on the plans to

ensure the adequacy of on-site provisions for each development stage. Snow storage plan has been revised to show what is already existing and what is proposed with the next phase of development. They are graphically differentiated.

28. Phase II must provide dedicated snow storage areas that serve only the paved surfaces proposed in that phase and meet the requirements of Section 409.

Phase II snow storage requirements are met at the corner lot (Lot 2), however, the proposed Lot 3 Apartments does not meet the snow storage requirements on its own site, without the ability to store snow within the detention pond area located on Lot 1. Therefore, a variance request has been included per comment number 29 below.

29. The current snow storage approach appears to rely on the development-wide application thread mentioned, which is not explicitly permitted in the CS zone without a variance. To proceed with this approach, the applicant should include a variance request to Section 409.D.1 in the narrative. Section 409.D.3 requires that off-site or shared snow storage be permitted only with an approved snow hauling plan that specifies receiving locations and haul frequency. In the variance narrative, please provide a snow management plan that includes details on long-term maintenance, snow hauling (if applicable), and access arrangements such as easements or plat notes to ensure long-term functionality.

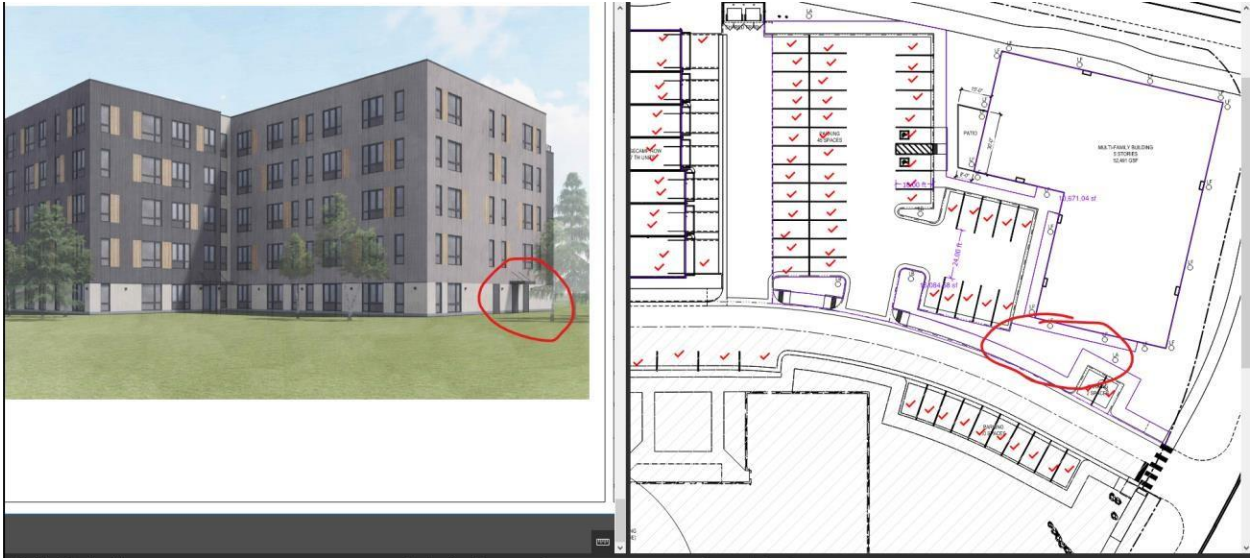
Variance to section 409.D.1 has been included in the narrative and proposes an alternative snow storage location of the detention pond on Lot 1 via a blanket snow storage easement that already exists for the subdivision (Steamboat Basecamp Easements Maintenance and Reimbursement Agreement, recorded at reception no. 861289).

Section 412 Critical Improvements

30. Draft Condition of Approval: Access drive, driveway, and parking areas are considered a critical improvement and must be constructed and approved or accepted prior to issuance of a Certificate of Occupancy/Completion or approval of a Condo/Townhome Final Plat, whichever occurs first. **Noted.**
31. Draft Condition of Approval: Drainage improvements are considered a critical improvement and must be constructed and approved or accepted prior to issuance of a Certificate of Occupancy/Completion or approval of a Condo/Townhome Final Plat, whichever occurs first. **Noted.**

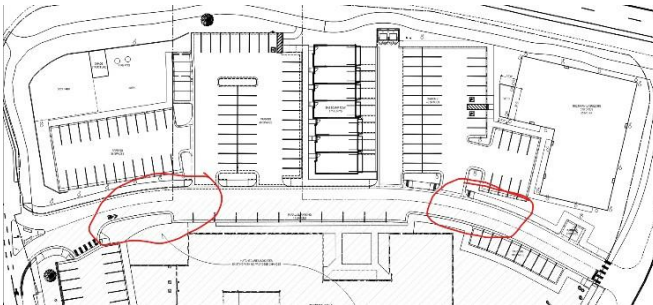
417 Internal Sidewalks

32. The internal sidewalk system is required to provide access to transit facilities, public gathering locations, and major site amenities. It appears that a sidewalk connection is missing from the entrance on the west side of the multifamily building (see markup). Please revise the plan to include a direct pedestrian connection from this entrance to the Big Bend Drive sidewalk in order to meet this standard. **Two sidewalk connections have been added to the site plan at this area, one with an accessible ramp, and another with steps.**



33. What paths are pedestrians expected to take to cross Big Bend Dr?

Two wide striping sections have been added to the site plan in the red-circled areas below. In addition, the applicant also proposes Pedestrian X-ing signage in both directions at both locations. These will reduce out-of-direction travel for pedestrians and help alert drivers of the potential for pedestrian crossings.



Section 437 Multiple-Family Residential Building Design Standards

34. The narrative evaluates Section 438, Commercial and Mixed-Use Building Design Standards, which does not apply. Only one residential building is proposed; therefore, per Section 437.B, the Multiple-Family Residential Building Design Standards apply. Please remove Variance Request #2 to Sections 438.E.1.b and 438.E.1.c. **Variance Request #2 has been removed.**

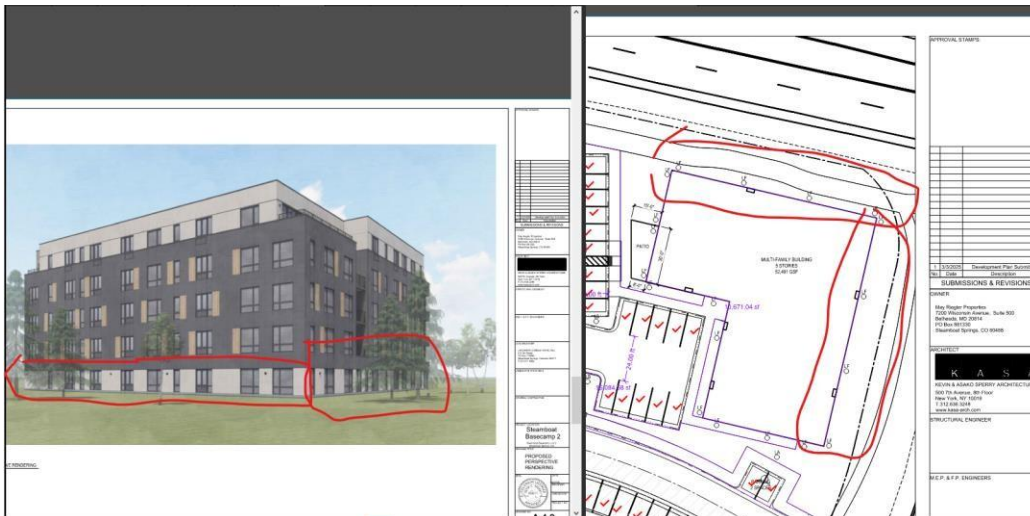
437.D Access

35. Per Section 437.D.1, buildings shall prioritize the orientation of primary pedestrian entries toward predominant public and private streets, pedestrian circulation routes, and gathering areas. Based on the current site and elevation plans, it does not appear that the multifamily building includes any pedestrian access or entries oriented toward Shield Dr or Lincoln Ave. Please revise the design to provide a clear pedestrian connection or access point on these frontages or clarify how the current layout meets the intent of this standard.

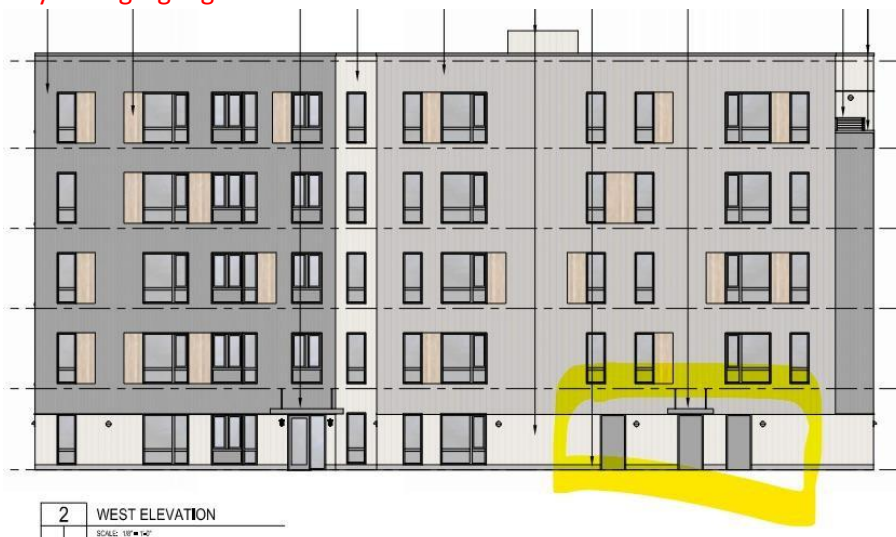
The building prioritizes the orientation of the primary pedestrian entry toward the predominant **PRIVATE street**, namely Big Bend Dr. Lincoln Ave/US40 is a high-traffic highway and not a pedestrian friendly access point. Curve Ct. is effectively a vehicular connector between US40 and Shield Dr, and also does not serve as a primary pedestrian route. The intent of the Basecamp site design is to bring pedestrians into the site via Big Bend Dr., which is a pedestrian friendly, private drive located immediately adjacent to

The bus stop and experiences virtually no cut-through vehicular traffic from non-residents or those not coming to the site for the commercial outlets. All of the resident parking for the proposed apartment building is oriented toward Big Bend drive so it makes it much more convenient for building users to have the building's primary pedestrian orientation/connection/entry along Big Bend Dr.

That said, in an effort to provide some orientation and connection to US40, we have added a secondary building entrance along the North elevation adjacent to the patio amenity space, which will contain a ramped sidewalk that connects to the US40 sidewalk. This entrance will have an elongated canopy that will provide a sense of entry and connection to vehicular traffic and pedestrians travelling along on US40.



36. The primary entrance on the west elevation appears visually similar to the adjacent utility room door. While the awning provides some distinction, additional architectural emphasis is needed to clearly identify this as a main entry. The entrance highlighted below is actually an egress-only door from the code-required egress stairwell. Access will be controlled at this door, and will only be available for residents, therefore we do not want to call too much attention to it only to confuse the general public. The main entrance is located plan west of this location and includes additional storefront/glazing to make it more pronounced. We plan to include wayfinding signage at this door that directs visitors to the main entrance around the corner.



437.F Roof Forms

37. The roof plan indicates a pitched roof with a 5:12 slope, which meets the standard for multiple-family buildings. However, the building elevations and section drawings appear to depict a flat or nearly flat roof. Please clarify the proposed roof design and ensure consistency across the plan set. If a flat roof is being proposed, a variance may be required. **VARIANCE?** A variance request has been included in the narrative.

437.H Building Scale, Variation, and Fenestration

38. Each façade of a multifamily building is required to incorporate variation in detail and fenestration by including at least three of the listed design elements (e.g., insets, balconies, material changes, etc.). While the east façade may meet this requirement, the remaining facades do not appear to provide sufficient variation to comply with this standard. Please revise all facades to demonstrate compliance with Section 437.H.1.a individually.

Facades have been revised with minor adjustments that have increased the variation in detail and fenestration. Each façade meets at least three of the design elements listed in 437.H.1.a as follows:

East Elevation:

- Balconies - Contains two vertical stacks of balconies, and terrace at 5th floor
- Variations in materials (2 different colors of cementitious wall planks, metal panel, accent panels at windows)
- Variations in relief (setbacks at top floor and balconies).
- Variations in window sizes (over 10 different window types/sizes)

North Elevation:

- Balconies - Contains one stack of balconies
- Variation in materials (2 different colors of cementitious wall planks, metal panel, accent panels at windows)
- Insets/relief (the building is L-Shaped and this elevation shows both planes, half the elevation is set back approximately 44'. Insets at balconies as well.
- Projection of canopies at ground floor entrances.
- Variations in window sizes (7 different window types/sizes)

West Elevation:

- Variation in materials (3 different colors of cementitious wall planks, metal panel, accent panels at windows)
- Insets/relief (the building is L-Shaped and this elevation shows both planes, half the elevation is set back approximately 62'). Relief at Balcony, 5th floor.
- Variations in window sizes (9 different window types)
- Projections of awnings/canopies at entrances

South Elevation:

- Variation in materials (2 different colors of cementitious wall planks, metal panel, accent panels at windows)
- Insets/relief with balconies at 5th floor units.
- Variations in window sizes (8 different window types)

39. Please update Variance Request #3 to reference the correct standard: Section 437.H.4.b, Multiple-Family Design Standards – EC Overlay Transparency. The transparency requirements in this section are identical to those in the Commercial and Mixed-Use standards, but Section 437 is the applicable reference for this project. **Variance request #3 has been updated to reference**

the correct standard.

40. Several of the window assemblies appear to include opaque lower panels. Please clarify what these opaque areas are and whether they are being counted toward the required ground floor transparency square footage. These opaque panels are the exterior grill for the Packaged Terminal Air Conditioning (PTAC) units. They are NOT being counted toward the transparency square footage. Transparency calcs have been updated to reflect information included in Written Interpretation WI-20-05 letter dated July 19, 2022. This update increased our transparency percentages as we previously had not been including the window frames in the in the transparency area (which are now included), and had been including the space between the finished ceilings and finished floor above in the overall wall/façade areas (which are now excluded).



437.J Building Color

41. The building appears to have a primary color of gray, which is not permitted in appendix B. Please revise. The grey fiber cement color of the building has been modified to a desaturated beige, and the metal panel changed to a charcoal color to better coordinate with the existing Lofts at Basecamp apartments across Big Bend Dr. Please see revised elevations.

437.K Mechanical, Service, and Accessory Structures

42. Please show roof top equipment to ensure compliance with 437.K.1. Roof top equipment is now shown on the roof plan, and screening in compliance with 437.K.1 has been added to the elevations and plan.