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May 24, 2024

City of Steamboat Springs
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Steamboat Springs, CO 80477
Phone: (970)-871-8207, Fax: (970)-879-8851

RE: Village Drive Apartments – Development -Plan at 142500001
Engineering Variance Request – Four Points Surveying and Engineering (FPSE)

Dear Engineering Department,

This narrative is a request for two engineering variances for the development of Village Drive Apartments at Pin No. 14250001 and PL20240114. The Mountain Office Park is a 1.28-acre subdivision and is zoned Multi-Family 3 (MF-3). The property is located on the northwest corner of Walton Creek Road and Village Drive. There is an existing two-story commercial office building constructed on the project. The proposed project is a remodel and preservation of the existing building and two new apartment buildings including 30% workforce housing development. There are two engineering variances being requested by this letter. The first request is to decrease the minimum turning radius for the proposed Internal Private Access (IPA) centerline and the second request pertains to a small portion of attached sidewalk

Variance #1

Standards to be Varied

The standard to be varied is from the City Engineering Standards per Chapter 4 Street Standards, Table 4-3. B – Internal Private Access Standards:

1. *Minimum Horizontal Curve along Centerline* → 61-feet
 - a. FPSE and the Owner are proposing a centerline radius of 35-feet

Site Specific Constraints

Four Points has encountered limitations in creating a roadway that meets the internal private access (IPA) standards due to our goal of remodeling the existing building on the site, which will be repurposed in the new development. The existing building is currently accessed from Village Drive via two access points, one to the south and one to the north, both of which terminate inside the property to the west.

Our proposed development includes a new residential building along the north and west perimeter of the lot, which will be accessed by a looped drive around the existing building, utilizing the current access points. Creating a roadway that meets the IPA standards from these existing driveways poses significant challenges. Specifically, a sixty-one-foot radius places the access too close to, or even inside, the existing building. Shifting the access further north would violate intersection standards at Walton Creek Road.

As shown in the attached exhibit, adhering strictly to the IPA standards would occupy the entire northern area, precluding any new development in that space. However, the remainder of the internal private access loop road will comply with IPA standards if the requested variance is granted.

Technical Reasoning & Sources

Practicality

The large existing building on the site needs a looped access with the tighter/smaller centerline radius to get traffic around the building while still providing room for additional development.

Consistency with Surrounding Development

The access drive proposed is consistent with the character of the multi-family residential developments in the area including a recent development at Urban Street at the Mountain, which has identical centerline radius constructed.

Fire Analysis

A fire truck turning analysis is provided with the plan set on page C7, outlining the safe turning movements through the site. This radius has previously been approved by the fire department for Urban Street at the Mountain and other similar developments.

Alternatives Considered

The 61-foot minimum centerline radius was considered and will not allow the area needed for usable development. An intersection option with stop signs at each proposed turn was also explored, but was determined impractical and would create unnecessary congestion.

Summary of Impacts

1. Safety – A fire turning analysis was conducted for the proposed access and included in the plan set. No impacts to fire operations were observed. Similar turning radius are utilized in similar developments in the area with no reported safety impacts.
2. Traffic Operations – Traffic operations will not be impacted as this is a relatively low volume access drive serving the proposed developments. Intersection control for this access will only reduce efficiency over what is proposed.

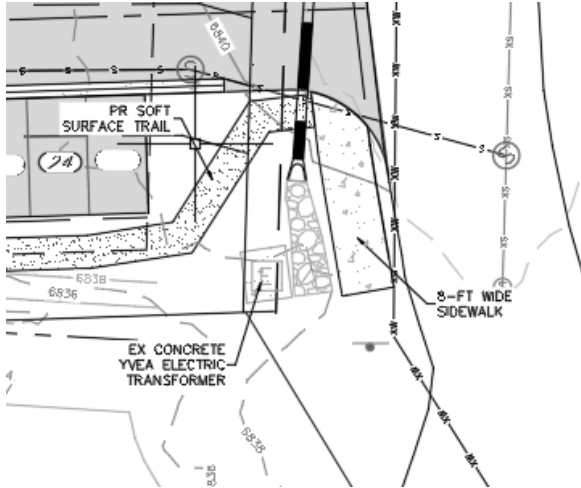
Mitigation Measures

Sharp curve signs could be installed if the City desires but no other mitigation measures were explored.

Variance #2

Standards to be Varied

The standard to be varied is from the City Engineering Standards per Chapter 4 Street Standards, Appendix 4-A, Connector – In-town or Neighborhood Street II, both of which call for various forms of landscape or drainage buffering between the sidewalk and edge of asphalt. FPSE and the Owner are requesting a small portion of the sidewalk along Village Drive to be attached, separated by a two-foot gravel buffer (see below). The attached area is located south of the south access entry point and is approximately 30 feet long.



Site Specific Constraints

There is an existing large, concrete YVEA transformer box directly in the path of where a typical cross section sidewalk would go. This is not a typical green electric transformer that can be easily relocated. It is a big concrete box which is a major junction and route for YVEA (see pic below). The cost for this transformer to be relocated would be \$30,000-50,000 which is impractical for constructing the unattached sidewalk associated with typical road cross sections. There will also be major hurdles finding a spot to relocate that will continue to allow use for all the properties that rely on this transformer.



Technical Reasoning & Sources

Consistency with Surrounding Development

There is an attached sidewalk directly north across Walton Creek Road along Village drive. See below.



New Location and Cost

There are a lot of properties in the area utilizing this transformer. A new location will affect all the surrounding properties and a large area for this transformer would be difficult to find. The cost associated with the relocation is also not practical for the application.

Drainage

The drainage from the area flows between the Sunray Meadows property to the south of the proposed Village Drive Apartments development. Relocation the transformer would be in an area that would disrupt this flow.

Alternatives Considered

Four Points reviewed two alternatives for relocation of the transformers.

1. Removing the Sidewalk from Development Plans. One alternative is to remove this portion of the sidewalk from the development plans. As required by the current development, the sidewalk will abruptly end at the south of the property, with no foreseeable future connection. The adjacent property to the south is already developed and occupied, with no sidewalk fronting Village Drive. The owner can post surety for this portion of the sidewalk, allowing it to be constructed once a connectivity plan is in place.
2. Attached Sidewalk with Curb. An attached sidewalk with a curb was considered, but there is no area for the point discharge to be received in the right-of-way (ROW). This is similar to Lot 24 Copper Ridge, where point discharge could not be collected in the ROW. To mitigate the absence of the curb, a 2-foot gravel buffer can be implemented.

Summary of Impacts

- a. Safety – Since there is already attached sidewalk in the area, with no known reported incidents, there will be no additional impact by granting this request.

Mitigation Measures

Gravel buffer between edge of asphalt and edge of sidewalk.

Conclusion

Alternatives were considered and evaluated as outline above. The proposed variances outlined in this letter will not adversely impact public safety. FPSE and the Owner feel that granting them will allow the development of the parcel to move forward in the most practical approach for additional housing in Steamboat Springs.

Please call or reply with any questions.

Sincerely;

Walter Magill, PE
Four Points Surveying and Engineering

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