



**STEAMBOAT
GONDOLA TRANSIT
CENTER**
Steamboat, CO

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only, and are not intended for
bidding or construction purposes.
12/20/23

KEY PLAN:

REVISIONS:

#	DATE	DESCRIPTION

DRAWN: LM REVIEWED: EG

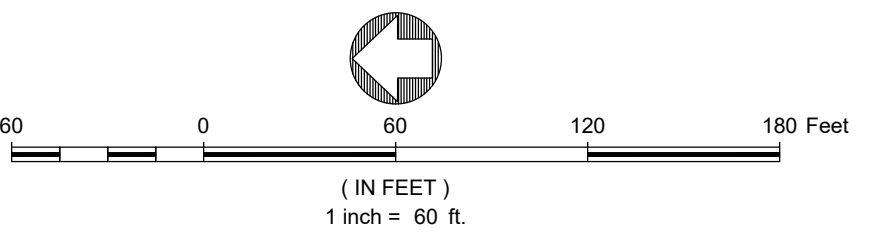
PROJECT NUMBER: 22077/1012-057
DATE: NOVEMBER 03, 2023

100%
SCHEMATIC
DESIGN
























DEMOLITION EXHIBIT

SHEET:

C.090

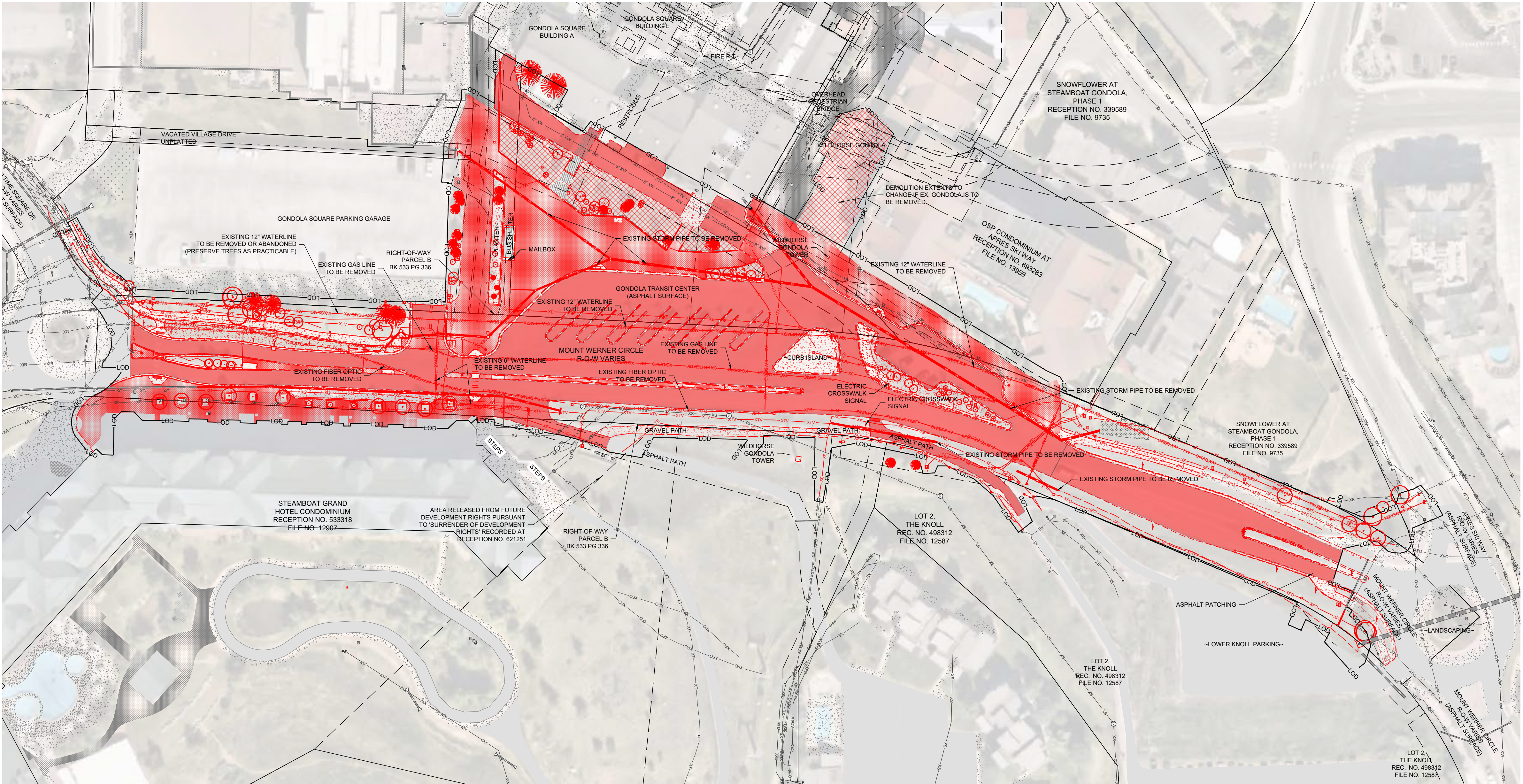


LEGEND:

- | | |
|---|---|
| EXISTING #7 SANITARY SEWER W/ MH & C.O. |  |
| EX. SANITARY SEWER TO BE REMOVED OR ABANDONED |  |
| EXISTING WATER |  |
| EX. WATER TO BE REMOVED OR ABANDONED |  |
| EXISTING OV & FH |  |
| EXIST #7 STORMCULVERT, INLET, MH, END SECTION WITH RIPRAP |  |
| EX. STORMCULVERT TO BE REMOVED OR ABANDONED |  |
| EXISTING FIBER OPTIC |  |
| EX. FIBER OPTIC TO BE REMOVED OR ABANDONED |  |
| EXISTING TELEPHONE |  |
| EX. TELEPHONE TO BE REMOVED OR ABANDONED |  |
| EXISTING TELEPHONE |  |
| EX. TELEPHONE TO BE REMOVED OR ABANDONED |  |
| PROBABLE LIMITS OF DISTURBANCE
(SUBJECT TO CHANGE BASED ON ENCOUNTERED
CONDITIONS AND CONTRACTOR MEANS/METHODS) |  |
| EXISTING ASPHALT |  |
| EXISTING ASPHALT TO BE REMOVED |  |
| EXISTING CONCRETE |  |
| EXISTING CONCRETE TO BE REMOVED |  |
| EXISTING GRAVEL |  |
| EXISTING GRAVEL TO BE REMOVED |  |
| EXISTING PAVERS |  |
| EXISTING PAVERS TO BE REMOVED |  |
| EXISTING WILDHORSE GONDOLA ARA TO BE REMOVED |  |
| GENERAL: ITEMS SHOWN IN RED ARE ANTICIPATED FOR DEMOLITION (LINE TYPES MAY VARY FROM THE ABOVE) | |

DEMOLITION NOTES:

1. THE SIZE, TYPE AND LOCATION OF ALL KNOWN UNDERGROUND UTILITIES ARE APPROXIMATE WHEN SHOWN ON THESE DRAWINGS. IT SHALL BE THE RESPONSIBILITY OF THE DEVELOPER TO VERIFY THE EXISTENCE OF ALL UNDERGROUND UTILITIES IN THE AREA OF THE WORK. BEFORE COMMENCING NEW CONSTRUCTION, THE CONTRACTOR SHALL BE RESPONSIBLE FOR LOCATING ALL UNDERGROUND UTILITIES AND SHALL BE RESPONSIBLE FOR ALL UNKNOWN UNDERGROUND UTILITIES.
2. CONTRACTOR SHALL BE RESPONSIBLE FOR COORDINATING DEMOLITION REMOVAL, REPLACEMENT, AND DISPOSAL OF ALL FACILITIES AND MATERIALS.
3. CONTRACTOR IS ENCOURAGED TO PERFORM DEMOLITION IN A MANNER THAT MAXIMIZED SALVAGE, RE-USE, AND RECYCLING OF MATERIALS. THIS INCLUDES APPROPRIATE SORTING AND SEPARATION OF MATERIALS SUCH AS BRICK, CONCRETE, ASPHALT, AND BASE COURSE SHOULD BE RECYCLED IF POSSIBLE.
4. LIMITS OF STREET CUT ARE APPROXIMATE. FINAL LIMITS ARE TO BE DETERMINED IN THE FIELD BY THE CITY ENGINEER. ALL REPAIRS TO BE IN ACCORDANCE WITH CITY STANDARDS AND SPECIFICATIONS.
5. MILL AND OVERLAY OF STREET CUT AREAS MAY BE REQUIRED. CONTACT CITY ENGINEER FOR DETERMINATION.
6. CONTRACTOR SHALL COORDINATE SITE DEMOLITION OPERATIONS WITH ALL OTHER TRADES PERFORMING WORK ON THE PROJECT.
7. CONTRACTOR SHALL REPLACE, REPAIR AND/OR RESTORE TO ORIGINAL CONDITION, ALL BUILDINGS AND SITE IMPROVEMENTS, NOT DESIGNATED FOR REMOVAL, THAT ARE DAMAGED BY A RAILROAD PROJECT. CONTRACTOR SHALL BE RESPONSIBLE FOR ADDITIONAL COST TO OWNERS IF UTILITIES ARE DAMAGED. CONTRACTOR SHALL VERIFY REPLACEMENT REQUIREMENTS WITH UTILITY PROVIDERS AND ARRANGE FOR MATERIALS REPAIR.
8. QUANTITIES SHOWN ON DRAWINGS ARE APPROXIMATE AND SHALL BE VERIFIED IN THE FIELD.



STEAMBOAT GONDOLA TRANSIT CENTER

Steamboat, CO














Steamboat, CO

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MATERIALS LEGEND

	UNIT PAVING
	UNIT PAVING - SNOWMELT
	UNIT PAVING - VEHICULAR, SNOWMELT
	CONCRETE PAVING - VEHICULAR
	CONCRETE PAVING - VEHICULAR, SNOWMELT
	CONCRETE PAVING - PEDESTRIAN
	CONCRETE PAVING - PEDESTRIAN, SNOWMELT
	ASPHALT
	STONE SLAB
	CONCRETE STEPS
	CONCRETE SIDE WALL
	CONCRETE TERRACE
	ADJACENT PROPERTY - SNOWMELT

SNOWMELT BREAKDOWN - PROPOSED		
AREA NAME	MATERIAL	SQFT
WEST PLAZA	UNIT PAVERS, CONC. STAIRS, CONC. RAMPS	35,950
CENTER ISLAND, NORTH CROSSING, & DRIVE LANES	VEHICULAR - CONCRETE, UNIT PAVERS & CONC. RAMPS	27,215
SERVICE DRIVE & ADA SHUTTLE DROP-OFF	UNIT PAVERS	2,968
MULTI-USE PATH & SOUTH SHUTTLE WALKS	PEDESTRIAN - CONCRETE & CONC. RAMPS	13,687
NORTH TURNAROUND	VEHICULAR - CONCRETE	4,488
SOUTH TURNAROUND	VEHICULAR - CONCRETE	7,207
SOUTH SHUTTLE & DRIVE LANE	PEDESTRIAN & VEHICULAR CONCRETE	10,130
	TOTAL	101,645

SNOWMELT BREAKDOWN - OSP			
	AREA NAME	MATERIAL	SQFT
	ONE STEAMBOAT PLACE	UNIT PAVERS	8,154

SNOWMELT BREAKDOWN - GRAND		
AREA NAME	MATERIAL	SQFT
STEAMBOAT GRAND	UNIT PAVERS, CONC. RAMPS	11,957

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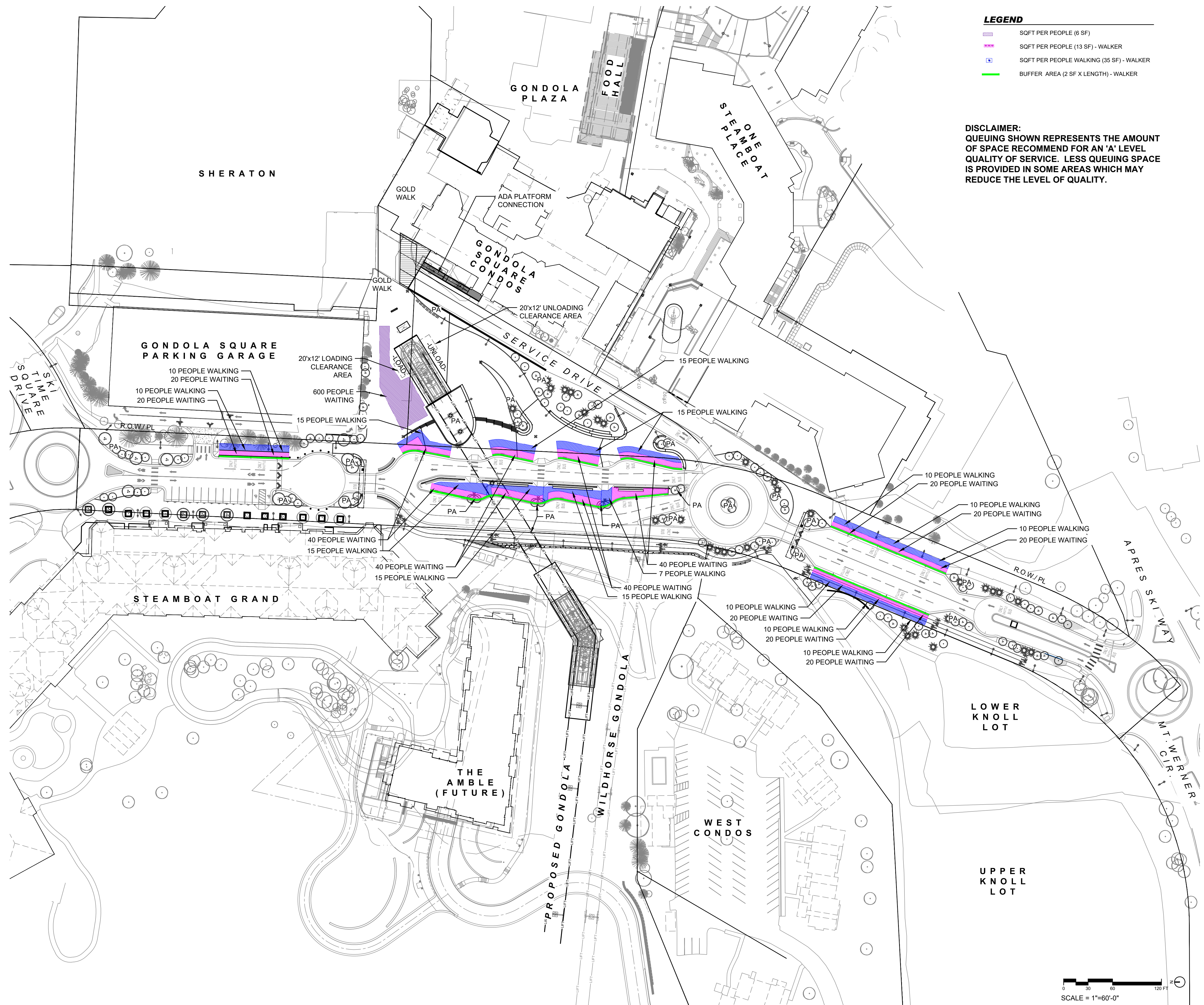
PROJECT NUMBER: 22077
DATE: DECEMBER 20, 2023

100%
SCHEMATIC
DESIGN

SNOWMELT AREA

SHEET:

EX.1



**STEAMBOAT SPRINGS GONDOLA TRANSIT CENTER
ANALYSIS, DATED AUGUST 21, 2023 BY WALKER
CONSULTANTS.**

(PAGES 19-21)

PLATFORM WAITING AND PEDESTRIAN AREAS
IN ADDITION TO AN INCREASED NUMBER OF TRANSIT
VEHICLE BERTHS, THE NEW GTC WILL FEATURE EXPANDED
PLATFORM AREAS FOR WAITING PASSENGERS AND
PEDESTRIANS WALKING TO AND FROM SHUTTLES.

GTC PLATFORM

THE MOST CONSTRAINED PLATFORM AREA FOR THE NEW GTC IS THE CENTER PLATFORM FOR FOUR OF THE EIGHT SST BERTHS, SINCE THESE ARE THE ONLY BERTHS WHERE BUSES ARE NOT LOCATED AT THE EDGE OF THE ROADWAY WHEN PASSENGERS ARE BOARDING AND ALIGHTING.

CENTER PLATFORM REQUIRED WAITING AREA

AT THE FOUR BERTHS WHICH ARE ALL INTENDED FOR FULL-SIZE SST TRANSIT BUSES, THERE ARE ASSUMED TO BE 40 BOARDINGS PER VEHICLE (FULL BUSES DURING PEAK HOUR IN THE AFTERNOON). AS A CONSERVATIVE ESTIMATE, IT IS ASSUMED THAT THE FOUR FULL-SIZE BERTHS AT THE NEW CENTER ISLAND PLATFORM WILL HAVE A MAXIMUM OF 160 PEOPLE WAITING TO BOARD. SINCE ALMOST ALL OF THESE PASSENGERS WILL BE CARRYING SKI GEAR, THE TCQM RECOMMENDS THE HIGHEST LEVEL OF SERVICE (LOS A) FOR THESE CIRCUMSTANCES, WHICH WOULD MEAN AT LEAST 13 SQUARE FEET PER PASSENGER. THE TOTAL COMBINED WAITING AREA ON THE CENTER ISLAND PLATFORM SHOULD BE AT LEAST $13 \times 160 = 2,080$ SQ FT.

CENTER PLATFORM REQUIRED WALKING AREA

IN ADDITION TO PEOPLE WAITING TO BOARD, THE CENTER PLATFORM SHOULD HAVE SPACE FOR PEDESTRIANS MOVING TOWARD THE AREA, WHICH IS EXPECTED TO BE PRIMARILY PASSENGERS ALIGHTING FROM BUSES OR WALKING TOWARD THEIR BUS BERTH FROM THE BASE AREA. FOR THOSE PEOPLE MOVING TOWARD THEIR BOARDING LOCATION, IT IS ASSUMED THAT THE WAITING AREA ALREADY PROVIDED ABOVE BASED ON A PLATFORM CAPACITY OF 160 PEOPLE WILL PROVIDE SUFFICIENT SPACE. FOR THOSE ALIGHTING FROM BUSES, WHO POTENTIALLY COULD BE IN ADDITION TO THE 160 PEOPLE WAITING ON THE PLATFORM, SOME ADDITIONAL PLATFORM SPACE IS WARRANTED, DESIGNING FOR THE PEAK UNKNOWN HOUR MEANS THAT THE DOMINANT PASSENGER FLOW IS COMING FROM THE BASE AREA AND BOARDING BUSES, AS WOULD BE EXPECTED AT THE END OF THE DAY. NEVERTHELESS, IT IS ASSUMED THAT SPACE FOR AN ADDITIONAL 60 ALIGHTING PASSENGERS (15 PER BUS) IS NEEDED ON THE PLATFORM. FOR LOS A, THESE 60 PEOPLE MOVING ON THE PLATFORM WOULD REQUIRE AN AVERAGE OF 35 SQ FT EACH, TO ACHIEVE LOS A, THE ADDITIONAL SPACE WOULD THEREFORE BE $35 \times 60 = 2,100$ SQ FT.

TOTAL RECOMMENDED CENTER PLATFORM SIZE

THE TOTAL RECOMMENDED SIZE OF THE PLATFORM IS THE SUM OF AREAS NEEDED FOR MOVING PEDESTRIANS AND FOR PASSENGERS WAITING TO BOARD DURING THE PEAK AFTERNOON HOUR, PLUS A BUFFER ZONE NEAR THE EDGES. THIS RECOMMENDED SIZE IS 2,080 (WAITING) + 2,100 (WALKING) + 400 (BUFFER) = 4,580 SQ FT. THE MOST RECENT DESIGN FOR THIS CENTER PLATFORM APPEARS TO PROVIDE ADEQUATE SPACE TO ACHIEVE LOS A.

OTHER BERTHS

SINCE THE OTHER BERTHS ARE AGAINST THE SIDE OF THE ROADWAY, PROVIDING ENOUGH SPACE FOR PEDESTRIANS AND WAITING PASSENGERS IS TYPICALLY EASIER. USING CALCULATIONS SIMILAR TO THAT SHOWN ABOVE TO ACHIEVE LOS A, THE FOLLOWING MINIMUM AREA IS RECOMMENDED PER BERTH:

SST AND LARGE BUS BERTHS

THE SIDEWALK AREA NEEDED FOR THESE FULL-SIZE TRANSIT BUSES IS CALCULATED AS FOLLOWS, BASED ON PEAK AFTERNOON HOUR ACTIVITY:

WAITING AREA = 40 PASSENGERS X 13 SQ FT PER PASSENGER = 520 SQ FT PER BERTH

WALKING AREA = 15 PASSENGERS X 35 SQ FT PER PASSENGER = 525 SQ FT PER BERTH

BUFFER AREA = 2 LINEAR FEET X 40 LINEAR FEET = 80 SQ FT PER BERTH

TOTAL = 520 + 525 + 80 = 1,125 SQ FT PER BERTH

LODGING SHUTTLE BERTHS

THE SIDEWALK AREA NEEDED FOR THESE SMALLER SHUTTLE VEHICLES IS CALCULATED AS FOLLOWS, BASED ON PEAK AFTERNOON HOUR ACTIVITY:

WAITING AREA = 20 PASSENGERS X 13 SQ FT PER PASSENGER = 260 SQ FT PER BERTH

WALKING AREA = 10 PASSENGERS X 35 SQ FT PER PASSENGER = 350 SQ FT PER BERTH

BUFFER AREA = 2 LINEAR FEET X 25 LINEAR FEET = 50 SQ FT PER BERTH

TOTAL = 260 + 350 + 50 = 660 SQ FT PER BERTH

IT SHOULD BE NOTED THAT THE ABOVE TOTALS DO NOT INCLUDE OTHER PEDESTRIANS PASSING THROUGH. THEREFORE, IF THE AREA WHERE TRANSIT VEHICLES ARE PICKING UP AND DROPPING OFF PASSENGERS IS ALSO USED BY OTHER PEDESTRIANS, THEN THE TOTAL AREA PER BERTH SHOWN ABOVE SHOULD BE PROVIDED AWAY FROM THE GENERAL FLOW OF PEDESTRIANS. IF THIS IS NOT POSSIBLE, ADDITIONAL SPACE SHOULD BE PROVIDED.

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QUEUING STUDY

SHEET:

EX. 2

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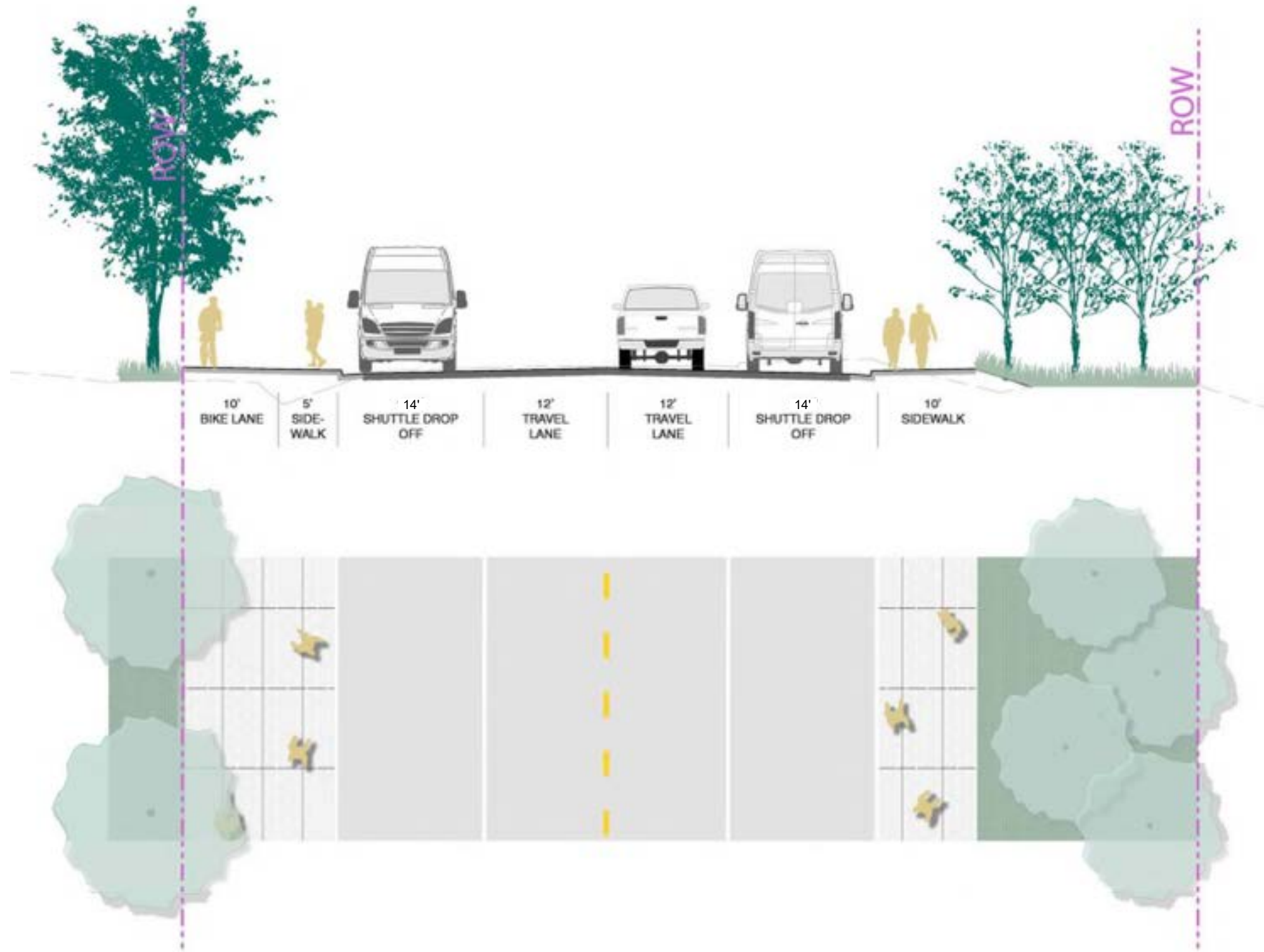
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100%
SCHEMATIC
DESIGN

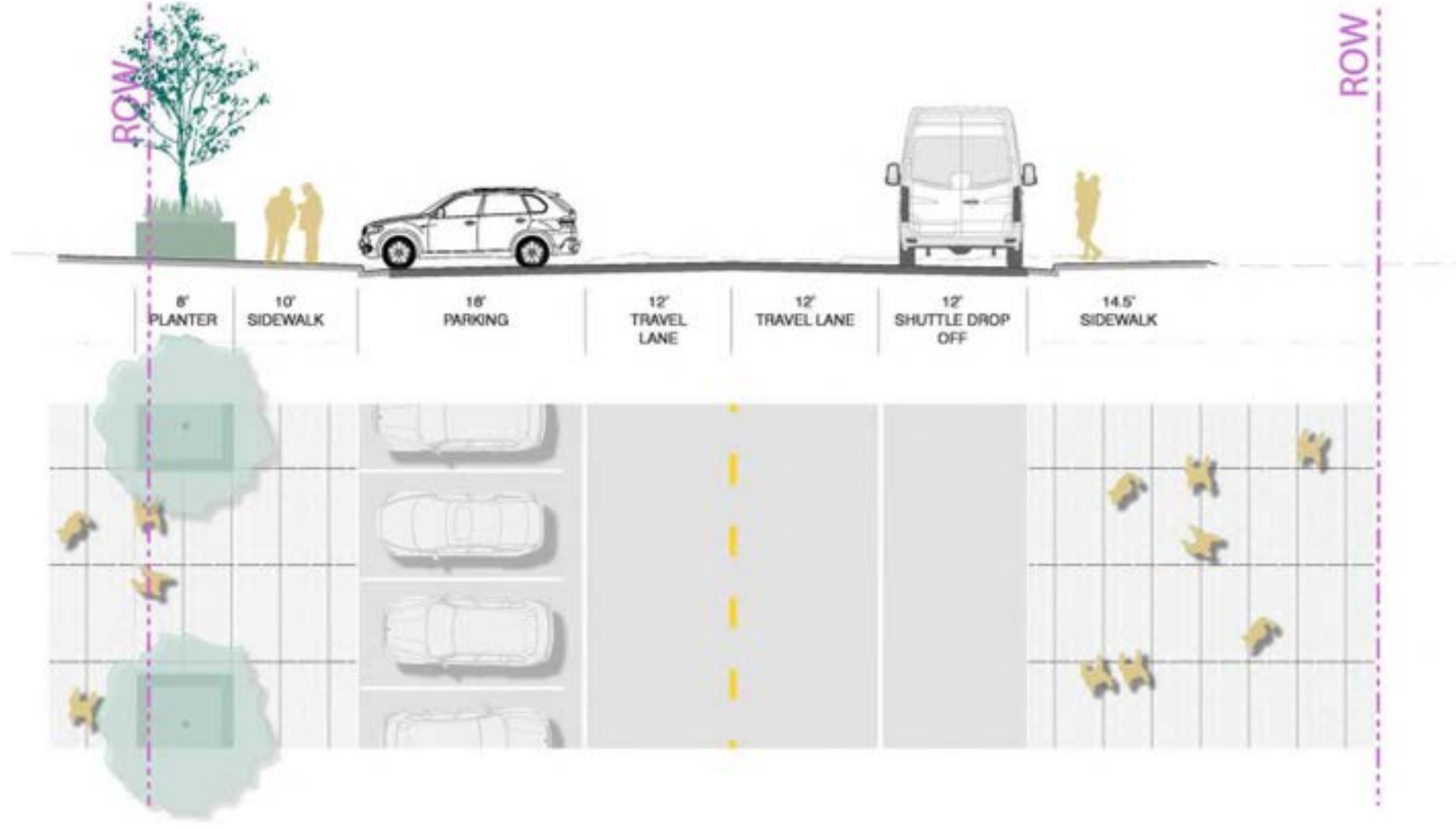
SITE CROSS
SECTIONS

SHEET:

L.200



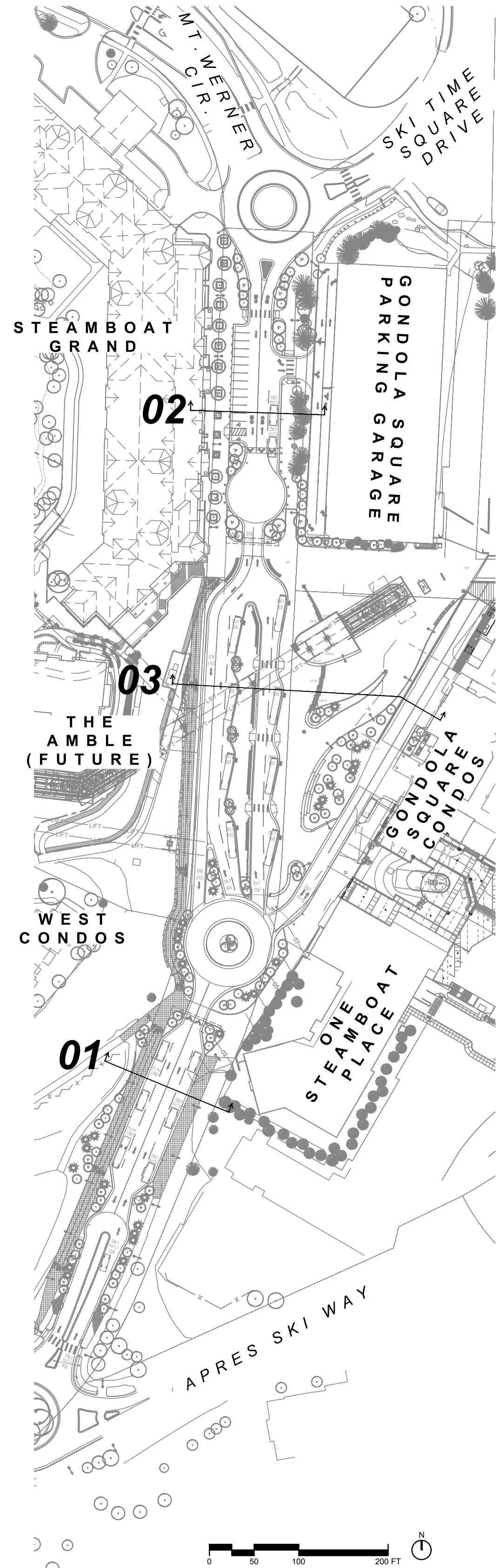
01 SHUTTLE DROP OFF SECTION
1" = 10'-0"



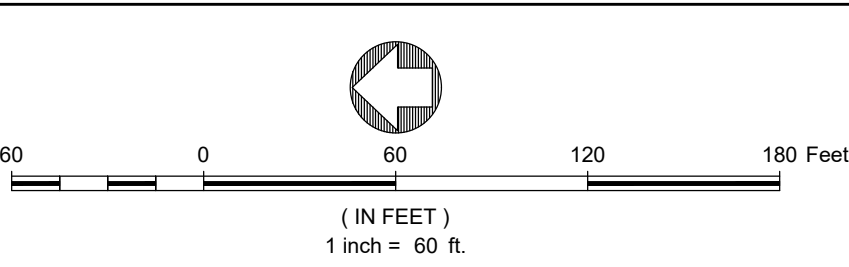
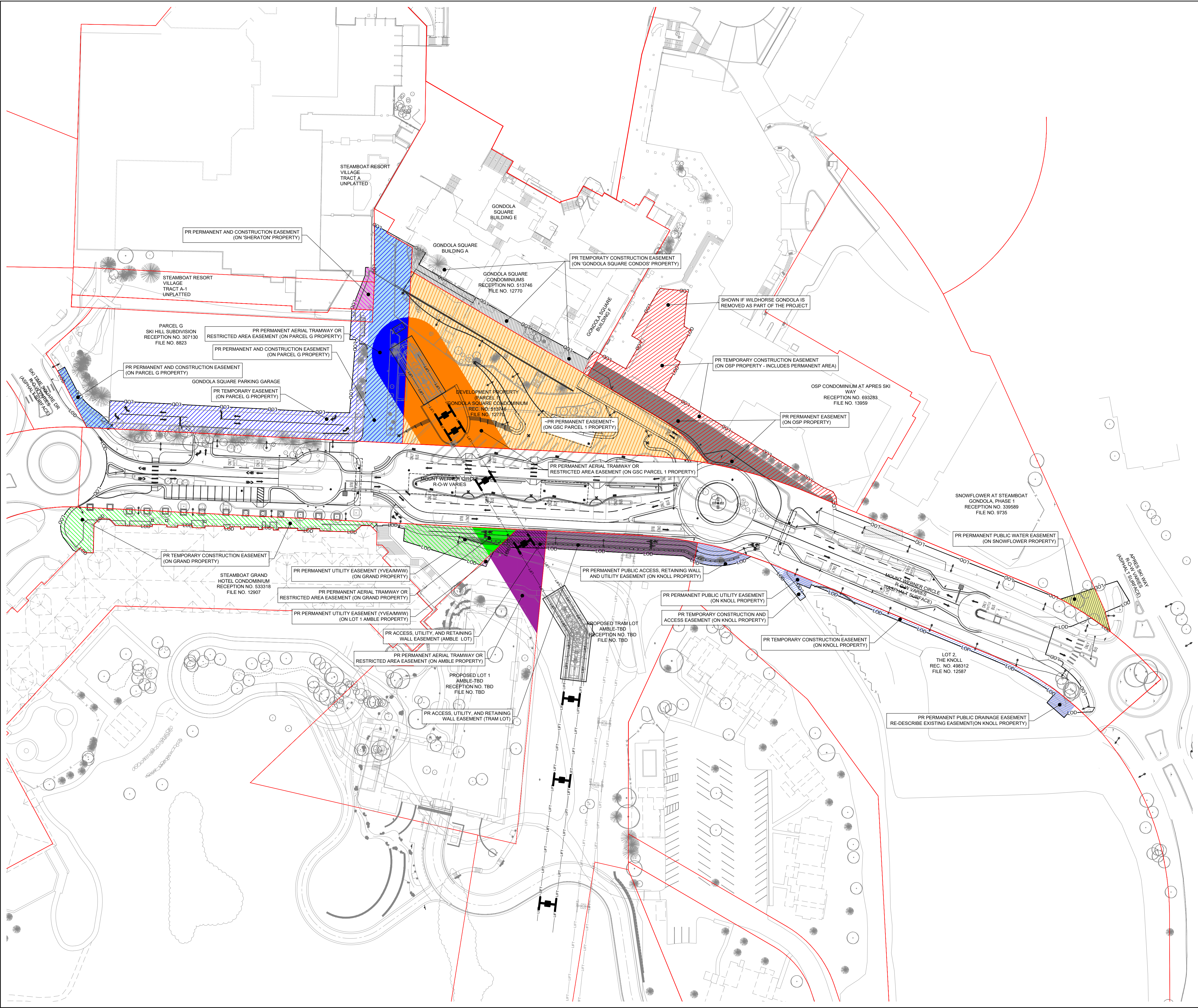
02 GRAND TO GONDOLA SQUARE SECTION
1" = 10'-0"



03 GTC SECTION
1" = 10'-0"



01 KEY MAP
1"=100'-0"



SITE PLAN LEGEND:

PROPERTY BOUNDARY
APPROXIMATE LIMITS OF DISTURBANCE

PARCEL G EASEMENTS (GONDOLA SQ. PARKING GARAGE)

PR TEMPORARY EASEMENT
PR PERMANENT EASEMENT
PR PERMANENT EASEMENT
TRAMWAY OR RESTRICTED AREA

'SHERATON' PARCEL EASEMENTS

PR PERMANENT EASEMENT

GRAND PARCEL EASEMENTS

PR TEMPORARY EASEMENT
PR PERMANENT EASEMENT
PR PERMANENT EASEMENT
TRAMWAY OR RESTRICTED AREA

GRAND PROPERTY (FUTURE AMBLE LOT 1)

PR ACCESS, RETAINING WALL
AND UTILITY EASEMENT
PR PERMANENT EASEMENT
TRAMWAY OR RESTRICTED AREA
PR PERMANENT EASEMENT

GRAND PROPERTY (PROPOSED TRAM LOT)

PR ACCESS, RETAINING WALL
AND UTILITY EASEMENT

GONDOLA SQUARE EXPANSION EASEMENTS (PARCEL 1)

PR PERMANENT EASEMENT
PR PERMANENT EASEMENT
TRAMWAY OR RESTRICTED AREA

'GONDOLA SQUARE CONDOS' PARCEL EASEMENTS

PR TEMPORARY EASEMENT

ONE STEAMBOAT PLACE PARCEL EASEMENTS

PR TEMPORARY EASEMENT
PR PERMANENT EASEMENT

KNOLL LOT 2' PARCEL EASEMENTS

PR TEMPORARY EASEMENT
PR PERMANENT EASEMENT

SNOWFLOWER' PARCEL EASEMENTS

PR PERMANENT EASEMENT



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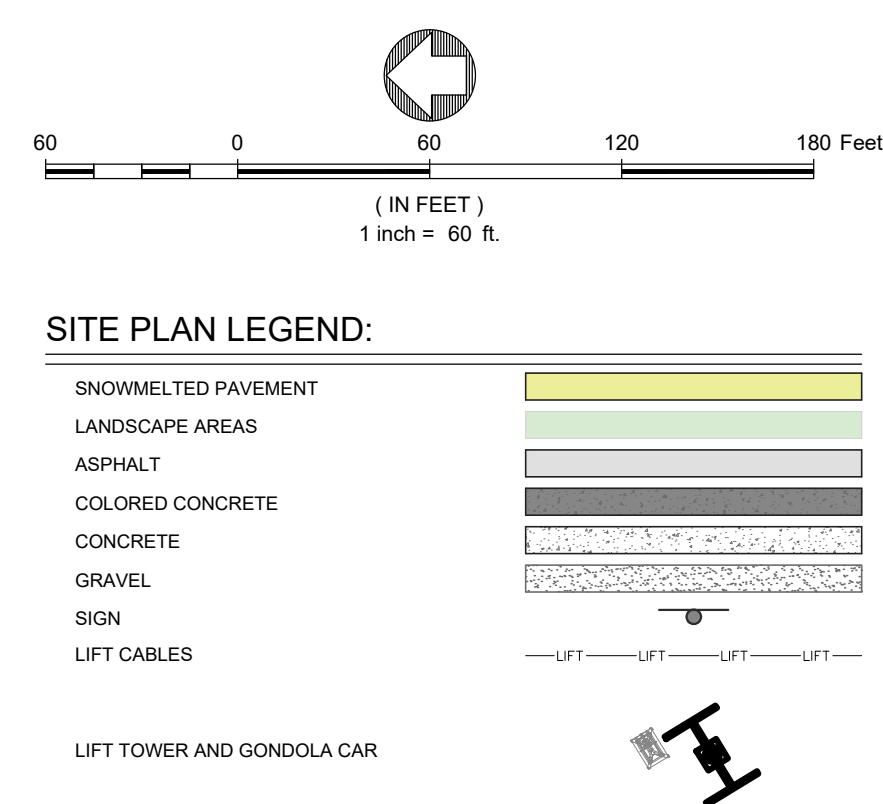
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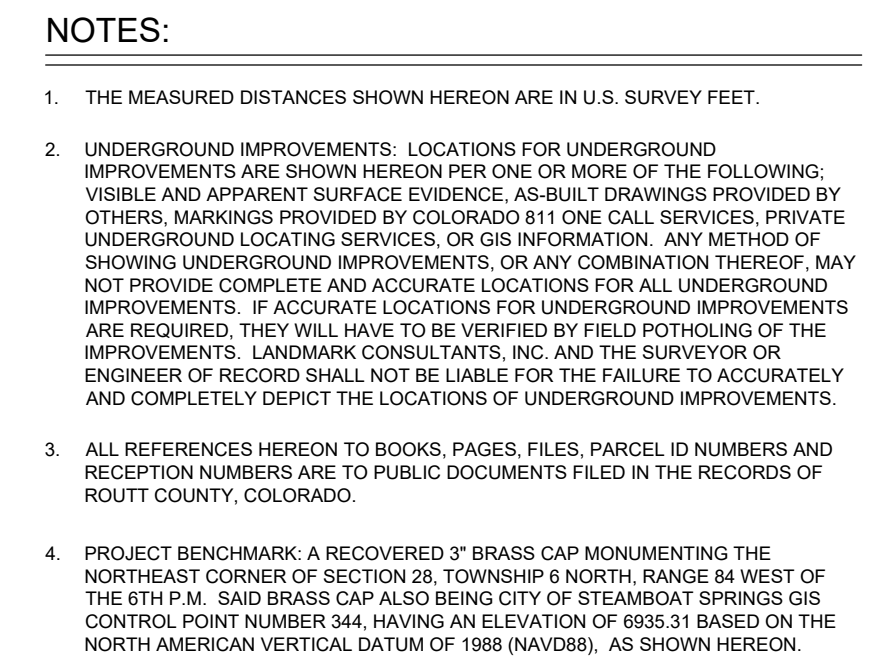
TEMPORARY-PERMANENT
EASEMENT PLAN

SHEET:

C.410



Steamboat, CO



KEY PLAN:

PROJECT NUMBER: 22077/1012-05
DATE: NOVEMBER 03, 2023

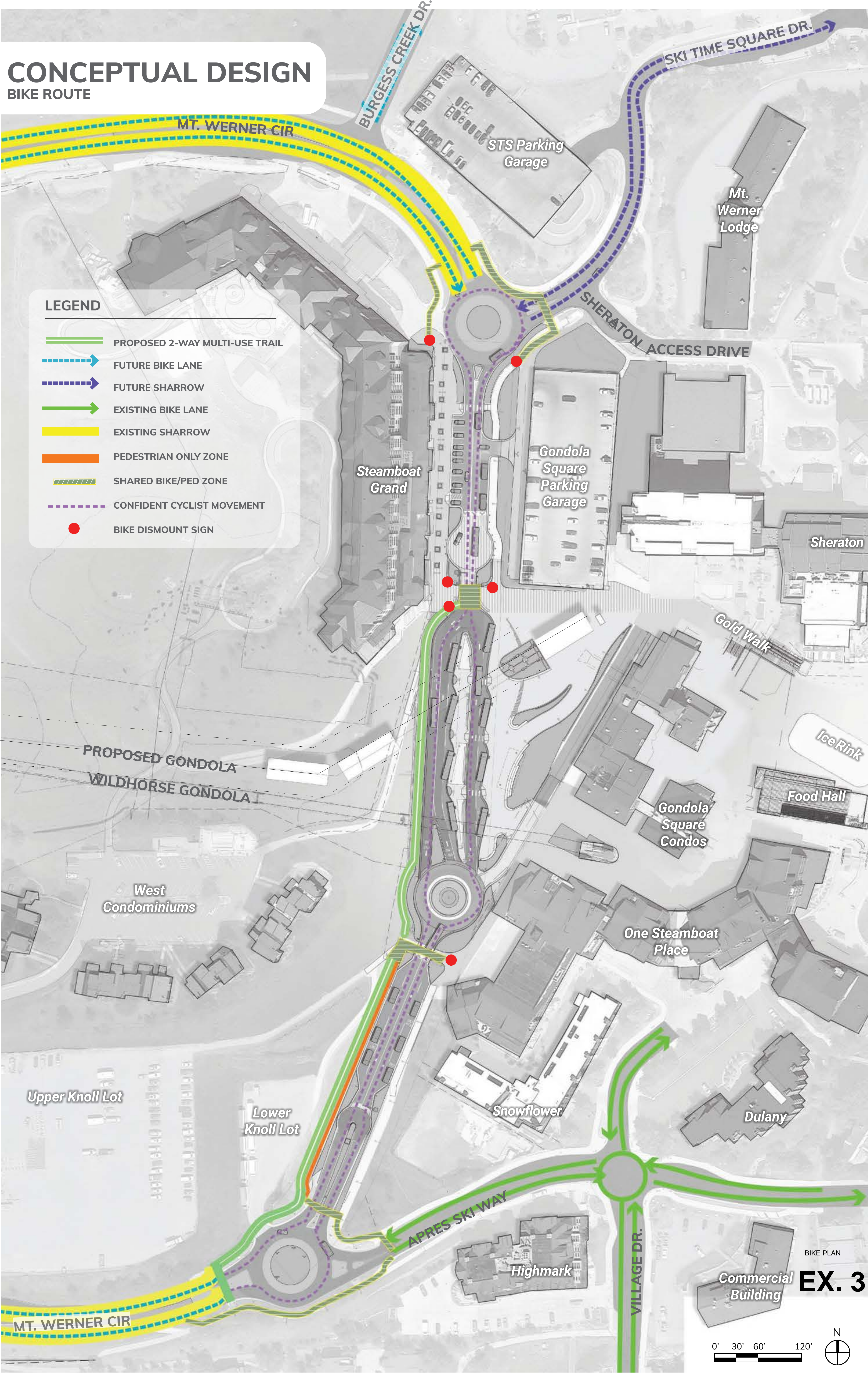
ROADWAY PLAN & PROFILE

SHEET:

C.400

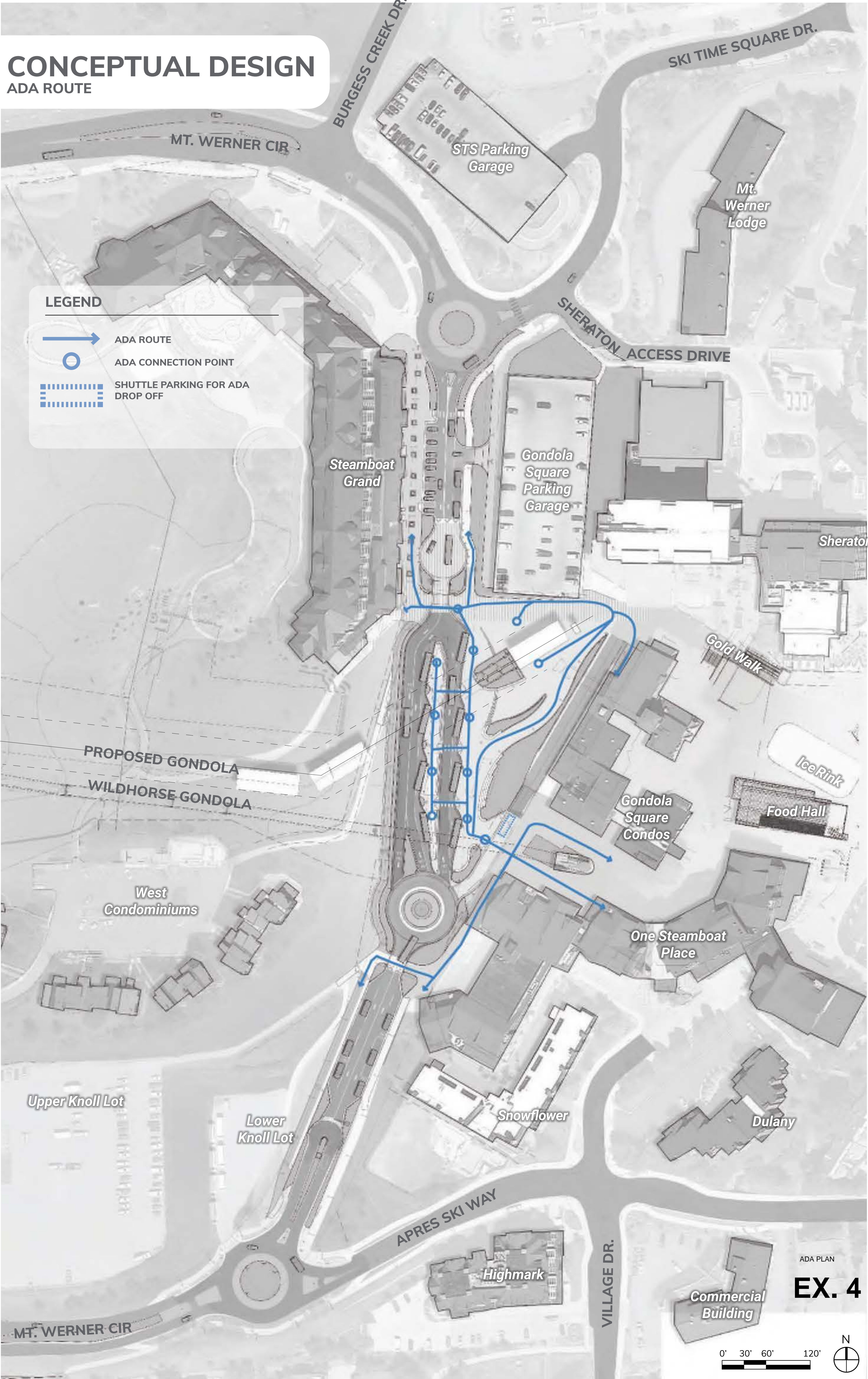
CONCEPTUAL DESIGN

BIKE ROUTE



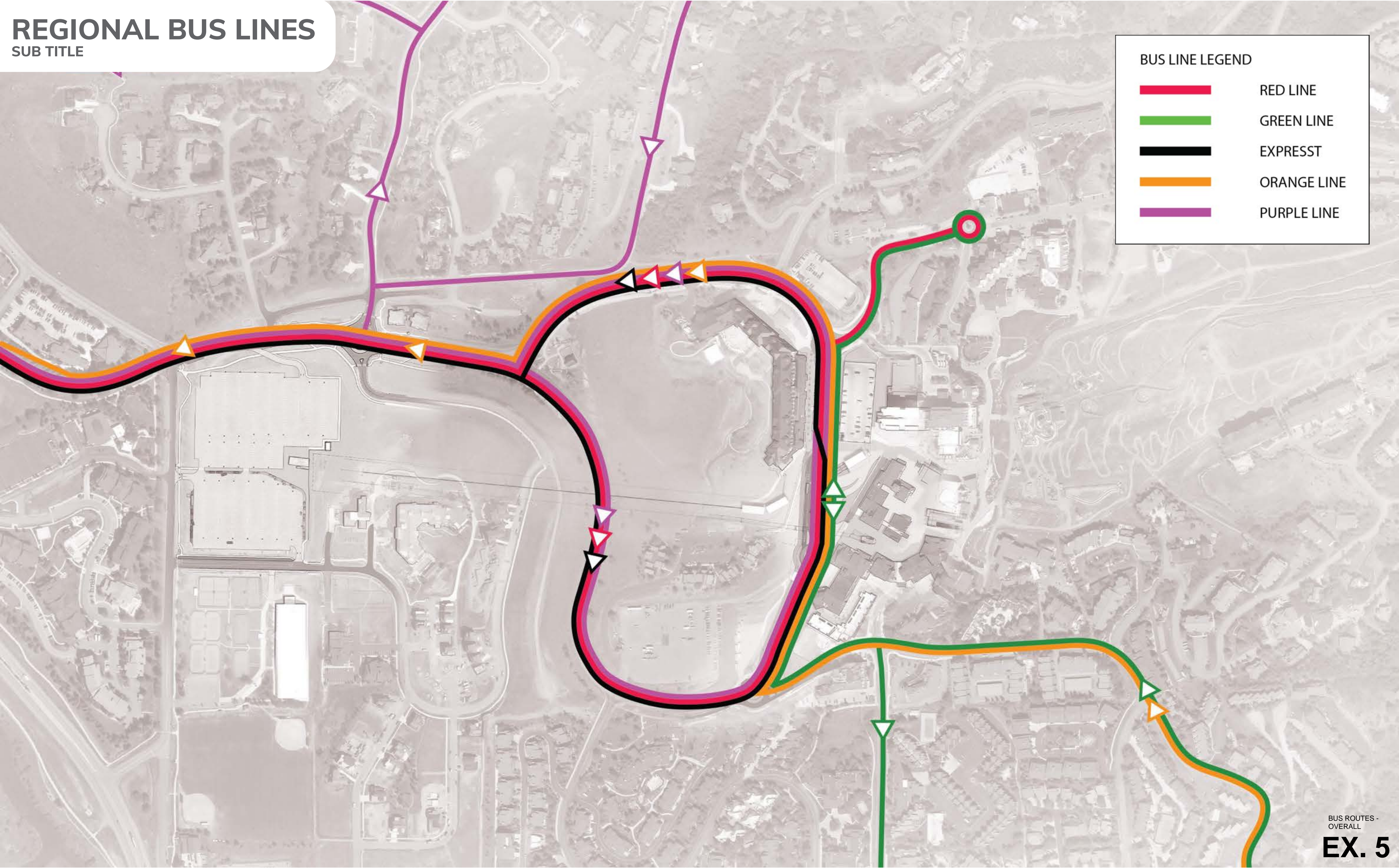
CONCEPTUAL DESIGN

ADA ROUTE



REGIONAL BUS LINES

SUB TITLE



BUS LINE LEGEND

RED LINE

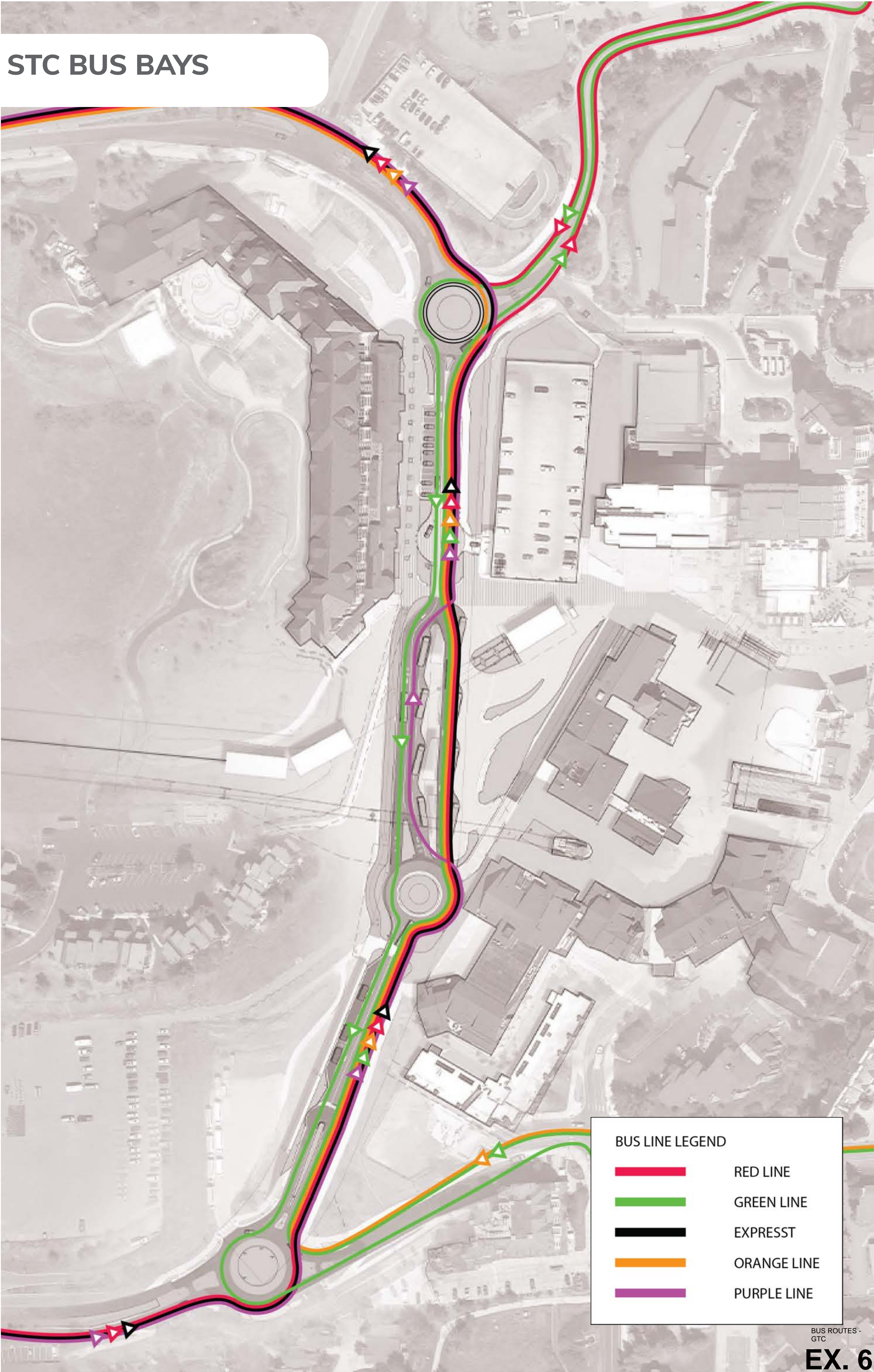
GREEN LINE

EXPRESST

ORANGE LINE

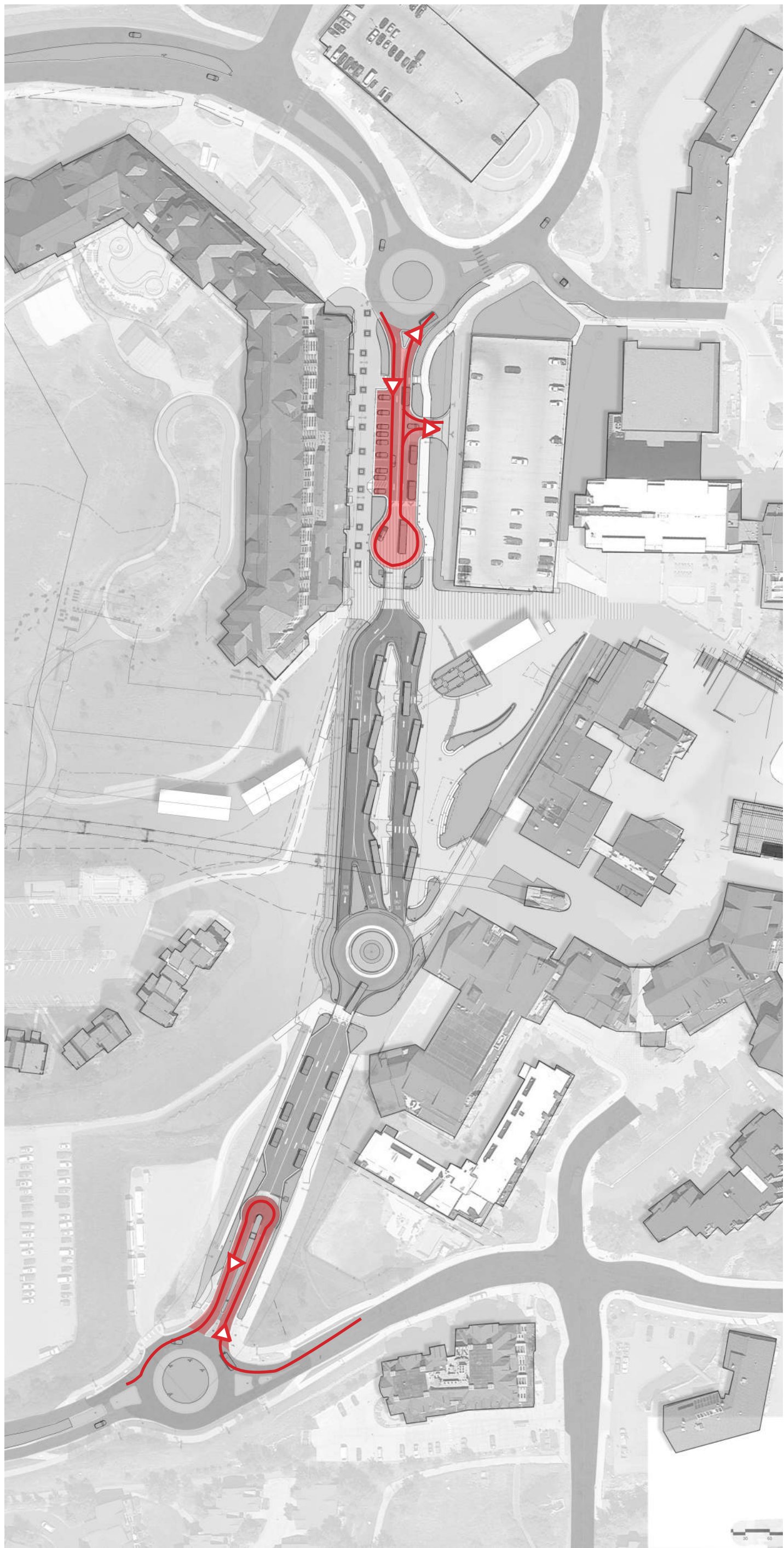
PURPLE LINE

STC BUS BAYS



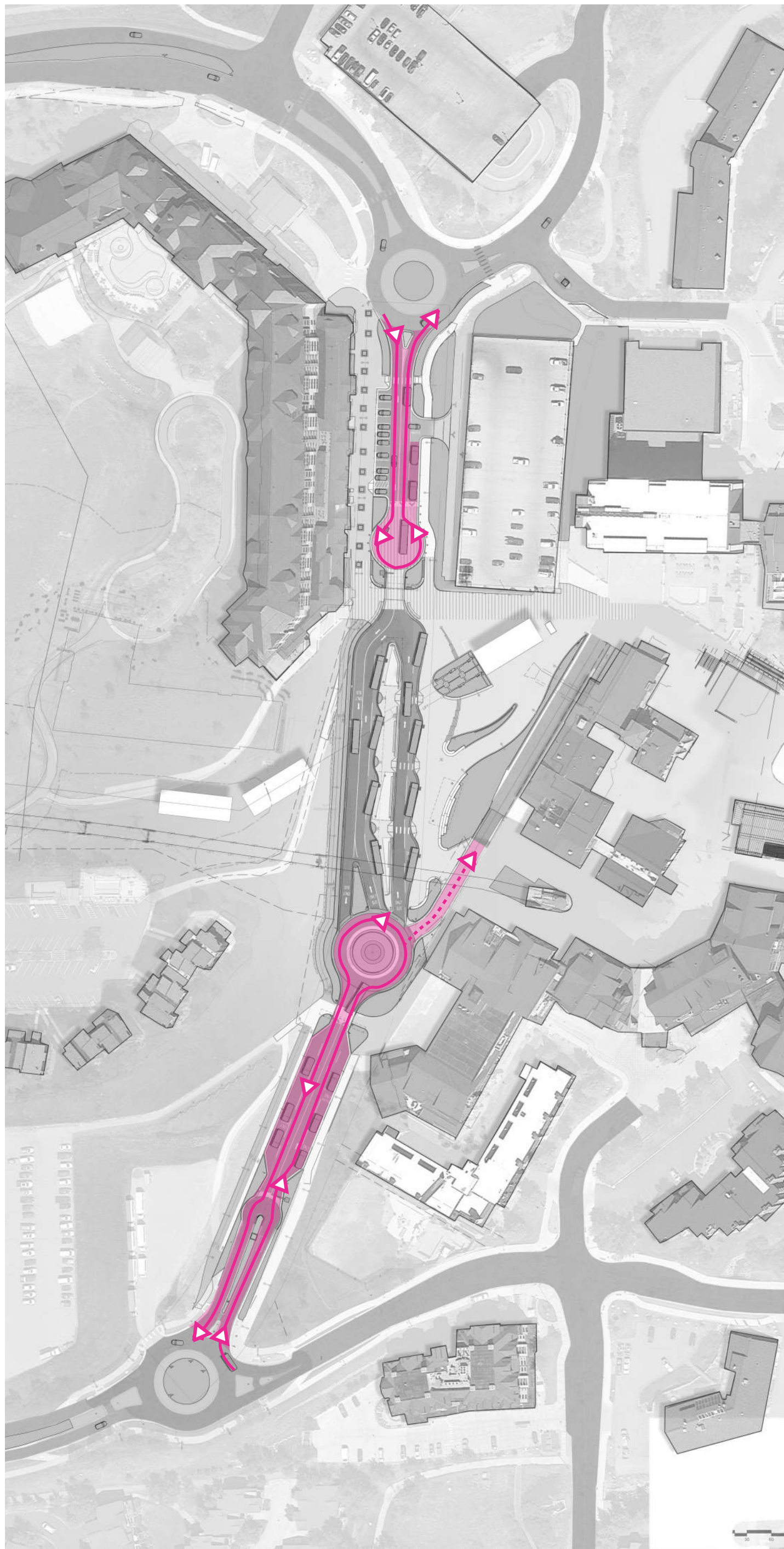
EX. 6

VEHICLE ACCESS



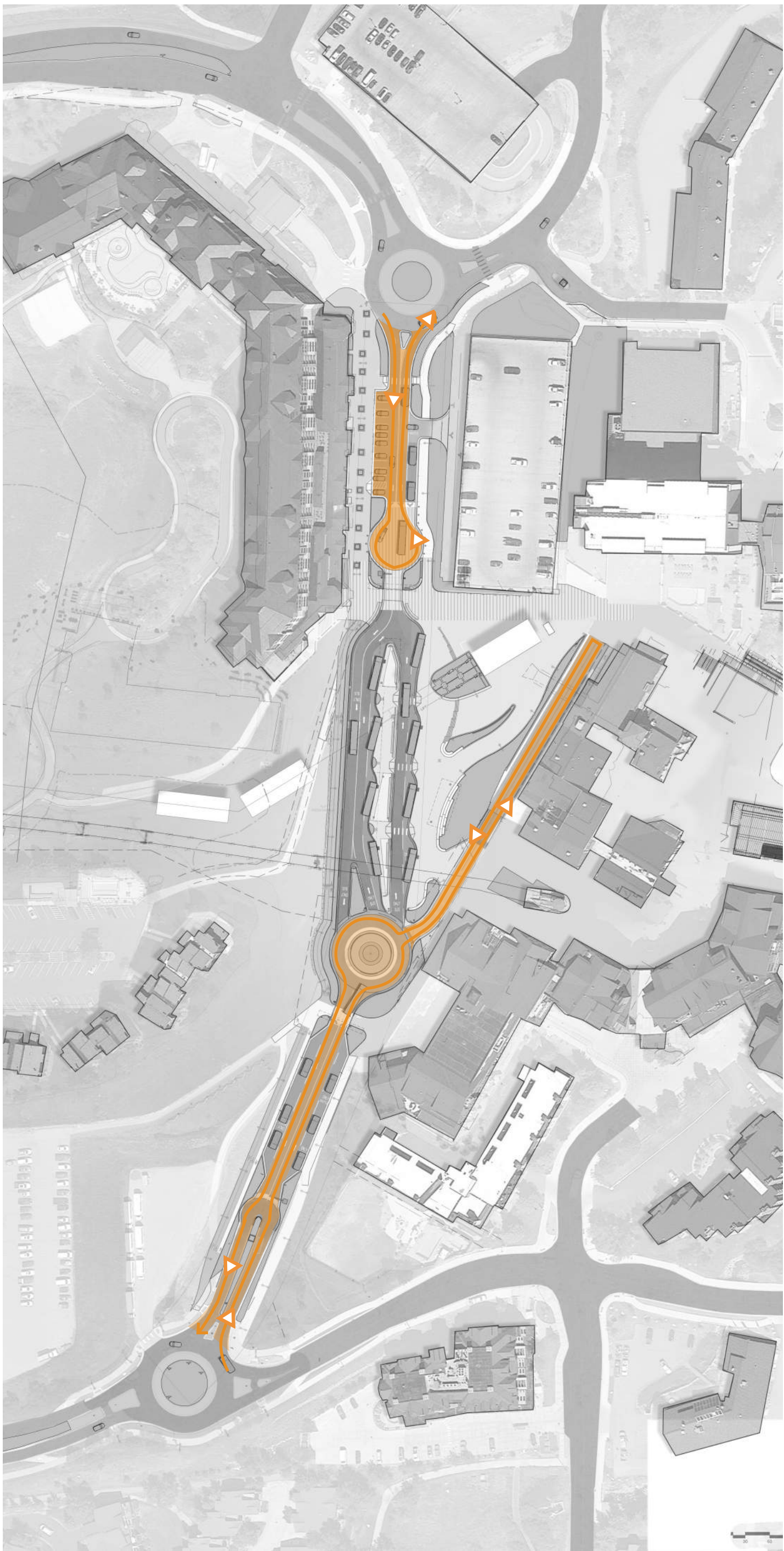
PRIVATE VEHICLE

- ACCESS AREA
- DIRECTION OF TRAVEL



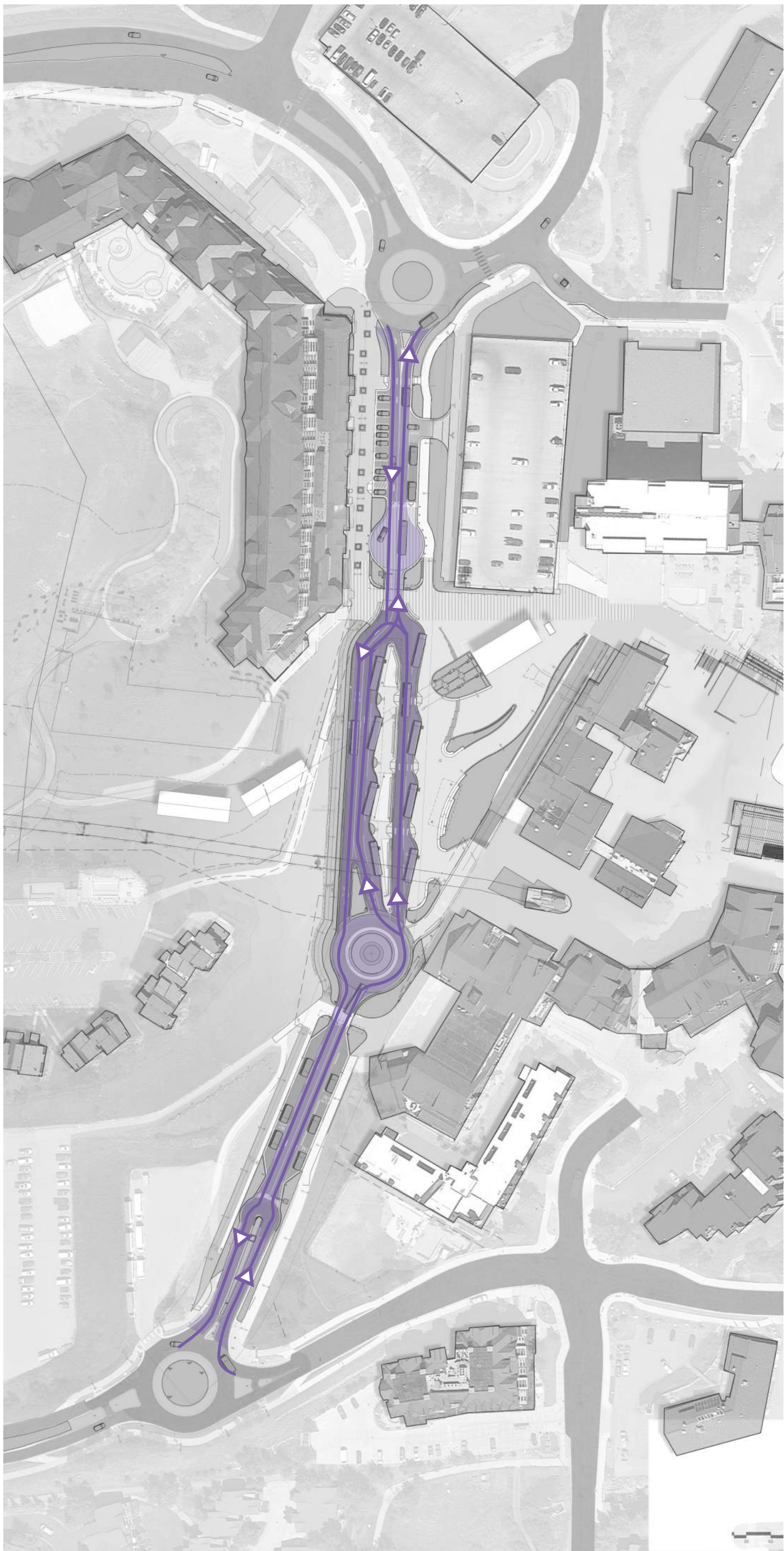
CONSOLIDATED SHUTTLE

- ACCESS AREA
- DIRECTION OF TRAVEL
- ADA ACCESS



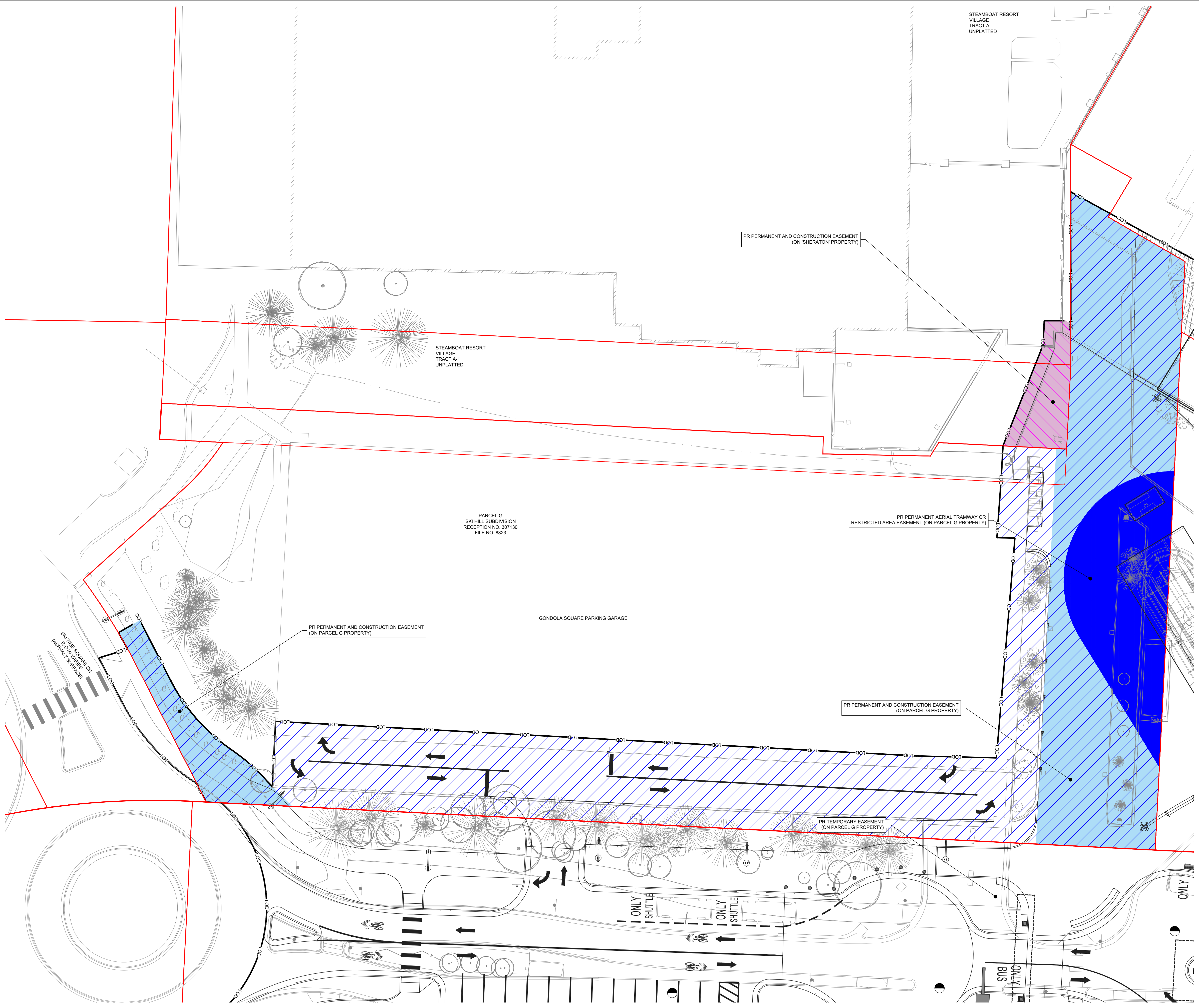
SERVICE VEHICLE / SPECIAL ACCESS

- ACCESS AREA
- DIRECTION OF TRAVEL



BUS

- ACCESS AREA
- DIRECTION OF TRAVEL



60060120180

(IN FEET)

1 inch = 60 ft.

SITE PLAN LEGEND:

PROPERTY BOUNDARY

APPROXIMATE LIMITS OF DISTURBANCE

LOD

PARCEL G EASEMENTS (GONDOLA SQ. PARKING GARAGE)

PR TEMPORARY EASEMENT

PR PERMANENT EASEMENT

PR PERMANENT EASEMENT TRAMWAY OR RESTRICTED AREA



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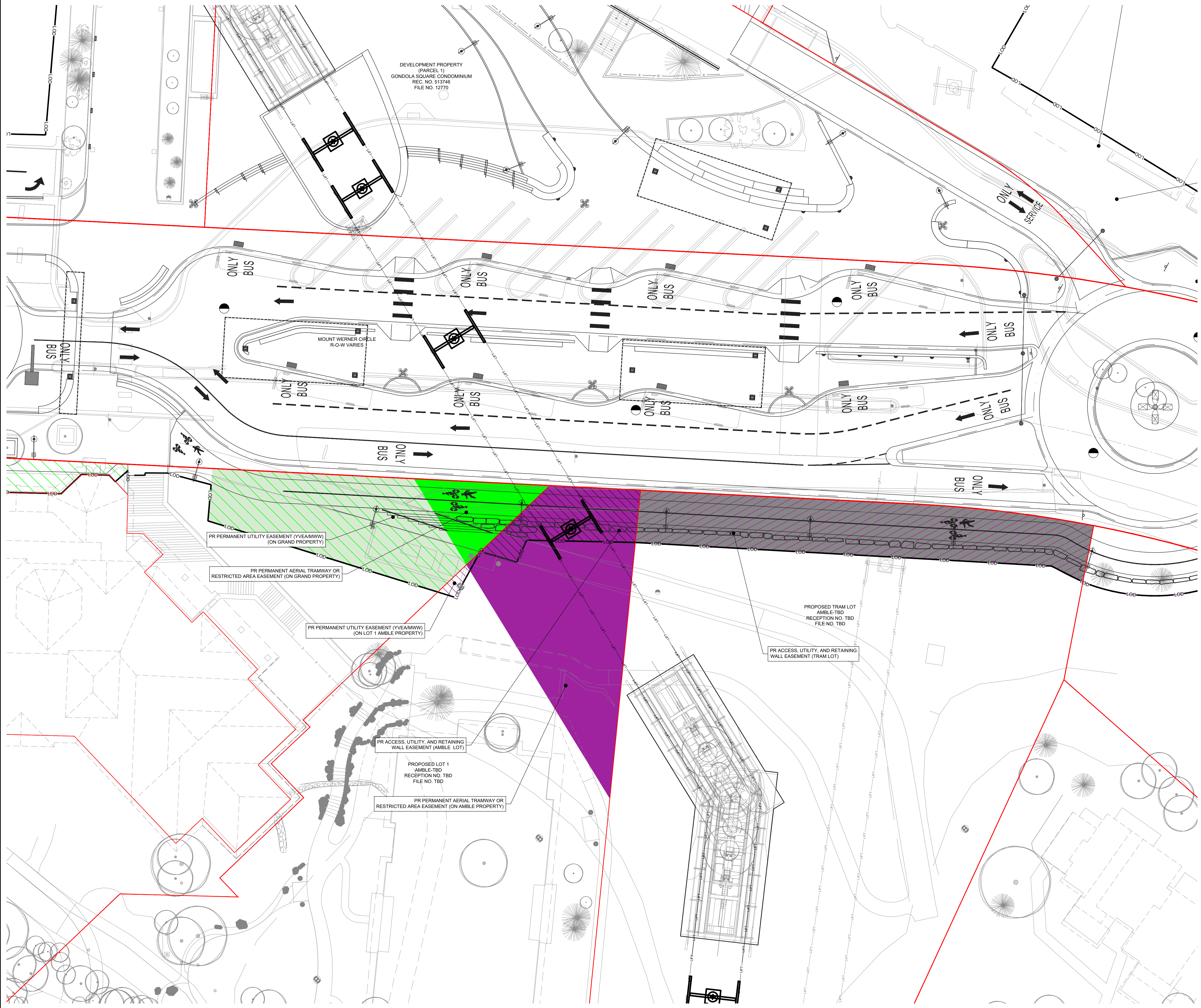
TEMPORARY-PERMANENT

EASEMENT PLAN

SHEET:

C.411





600

0

60

120

180 Feet

(IN FEET)
1 inch = 60 ft.

SITE PLAN LEGEND:

PROPERTY BOUNDARY

APPROXIMATE LIMITS OF DISTURBANCE

GRAND PARCEL EASEMENTS

PR TEMPORARY EASEMENT

PR PERMANENT EASEMENT

PR PERMANENT EASEMENT
TRAMWAY OR RESTRICTED AREA

GRAND PROPERTY (FUTURE AMBLE LOT 1)

PR ACCESS, RETAINING WALL
AND UTILITY EASEMENT

PR PERMANENT EASEMENT
TRAMWAY OR RESTRICTED AREA

PR PERMANENT EASEMENT

W e n k

ASSOCIATES
PLANNERS &
LANDSCAPE
ARCHITECTS

LANDMARK

CONSULTANTS, INC.

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TEMPORARY-PERMANENT
EASEMENT PLAN

SHEET:

C.413



60060120180

(IN FEET)

1 inch = 60 ft.

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SITE PLAN LEGEND:

PROPERTY BOUNDARY

APPROXIMATE LIMITS OF DISTURBANCE

LOO

GONDOLA SQUARE EXPANSION EASEMENTS (PARCEL 1)

PR PERMANENT EASEMENT

PR PERMANENT EASEMENT TRAMWAY OR RESTRICTED AREA

W e n k

ASSOCIATES

PLANNERS &

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EASEMENT PLAN

SHEET:

C.414



APPROXIMATE LIMITS OF DISTURBANCE

APPROXIMATE LIMITS OF DISTURBANCE ———— LOD

PR PERMANENT EASEMENT

100

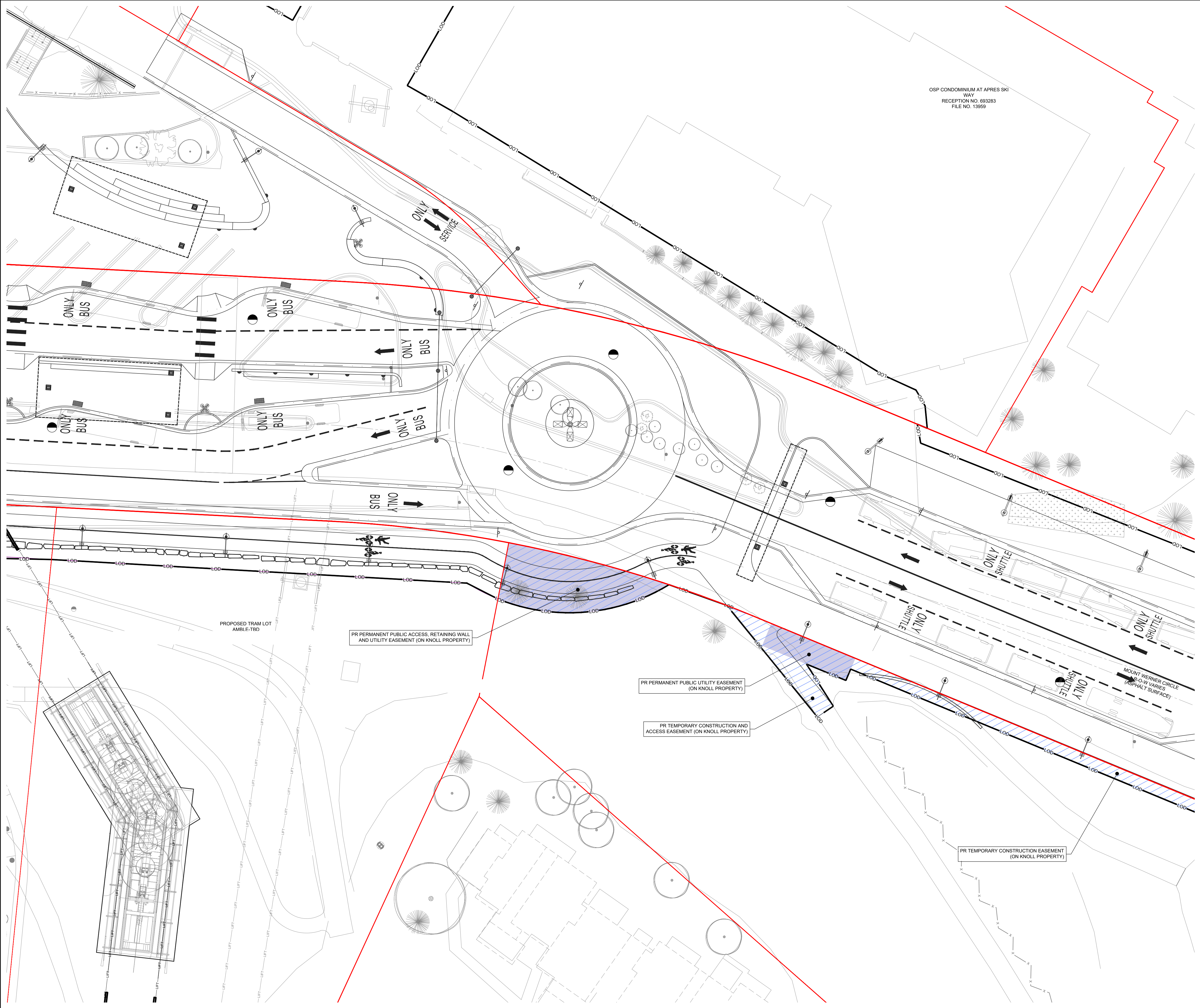
100

DRAWN: LM REVIEWED: EG

SHEET:

KEY PLAN:

C.415



600060120180

(IN FEET)

1 inch = 60 ft.

W

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n

k

SITE PLAN LEGEND:

PROPERTY BOUNDARY

APPROXIMATE LIMITS OF DISTURBANCE

W

e

n

k

ASSOCIATES

PLANNERS &

LANDSCAPE

ARCHITECTS



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KEY PLAN:

KNOLL LOT 2' PARCEL EASEMENTS

PR TEMPORARY EASEMENT

PR PERMANENT EASEMENT

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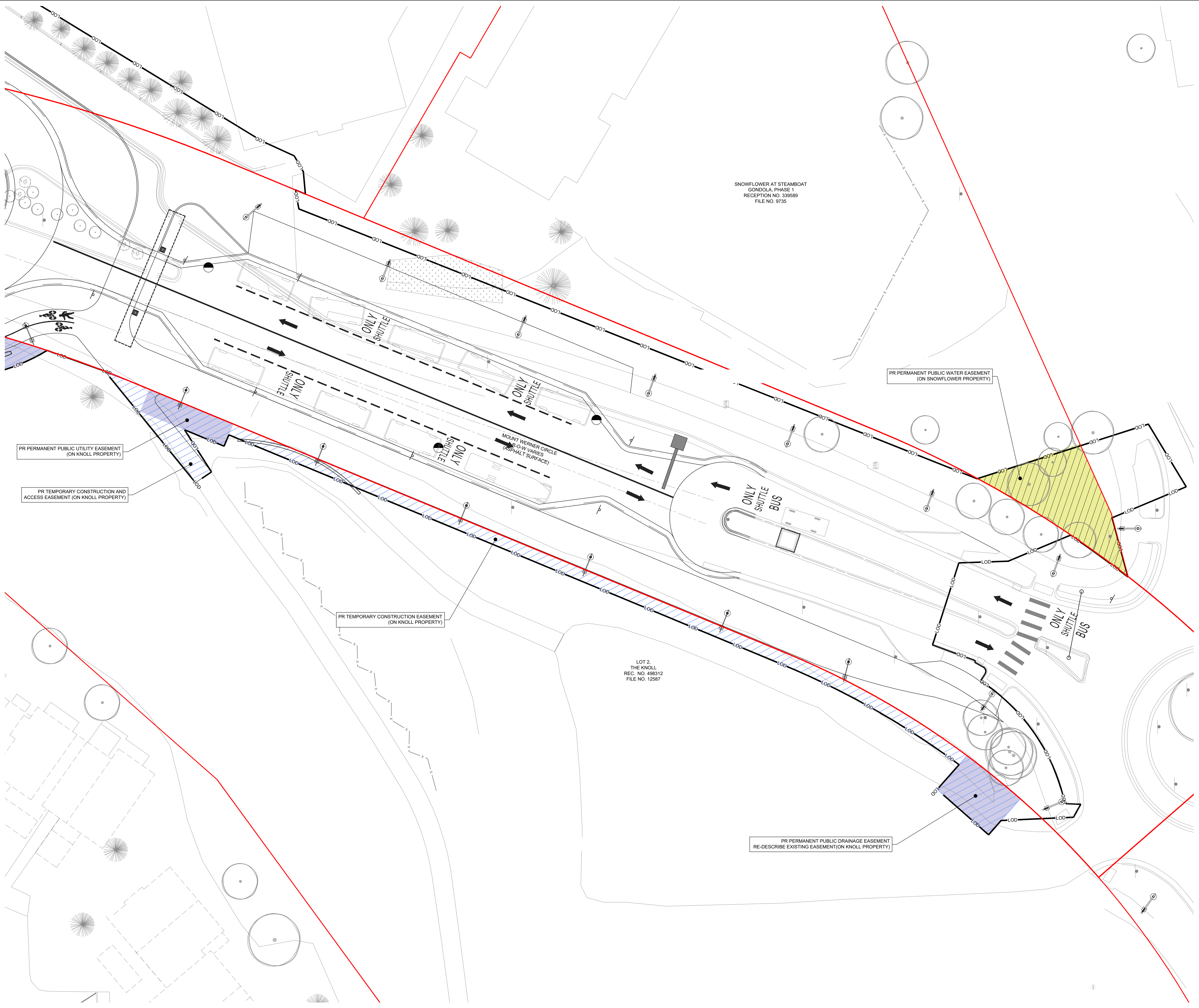
DATE: NOVEMBER 03, 2023

100% SCHEMATIC DESIGN

TEMPORARY-PERMANENT EASEMENT PLAN

SHEET:

C.416



600060120180

(IN FEET)

1 inch = 60 ft.

◀

▶

SITE PLAN LEGEND:

PROPERTY BOUNDARY

APPROXIMATE LIMITS OF DISTURBANCE

LOD

W e n k

ASSOCIATES
PLANNERS &
LANDSCAPE
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KEY PLAN:

KNOLL LOT 2' PARCEL EASEMENTS

PR TEMPORARY EASEMENT

PR PERMANENT EASEMENT

SNOWFLOWER' PARCEL EASEMENTS

PR PERMANENT EASEMENT

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DRAWN: LM REVIEWED: EG		

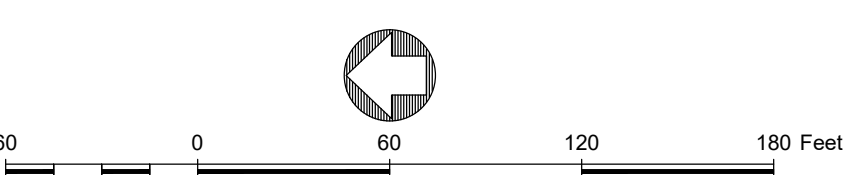
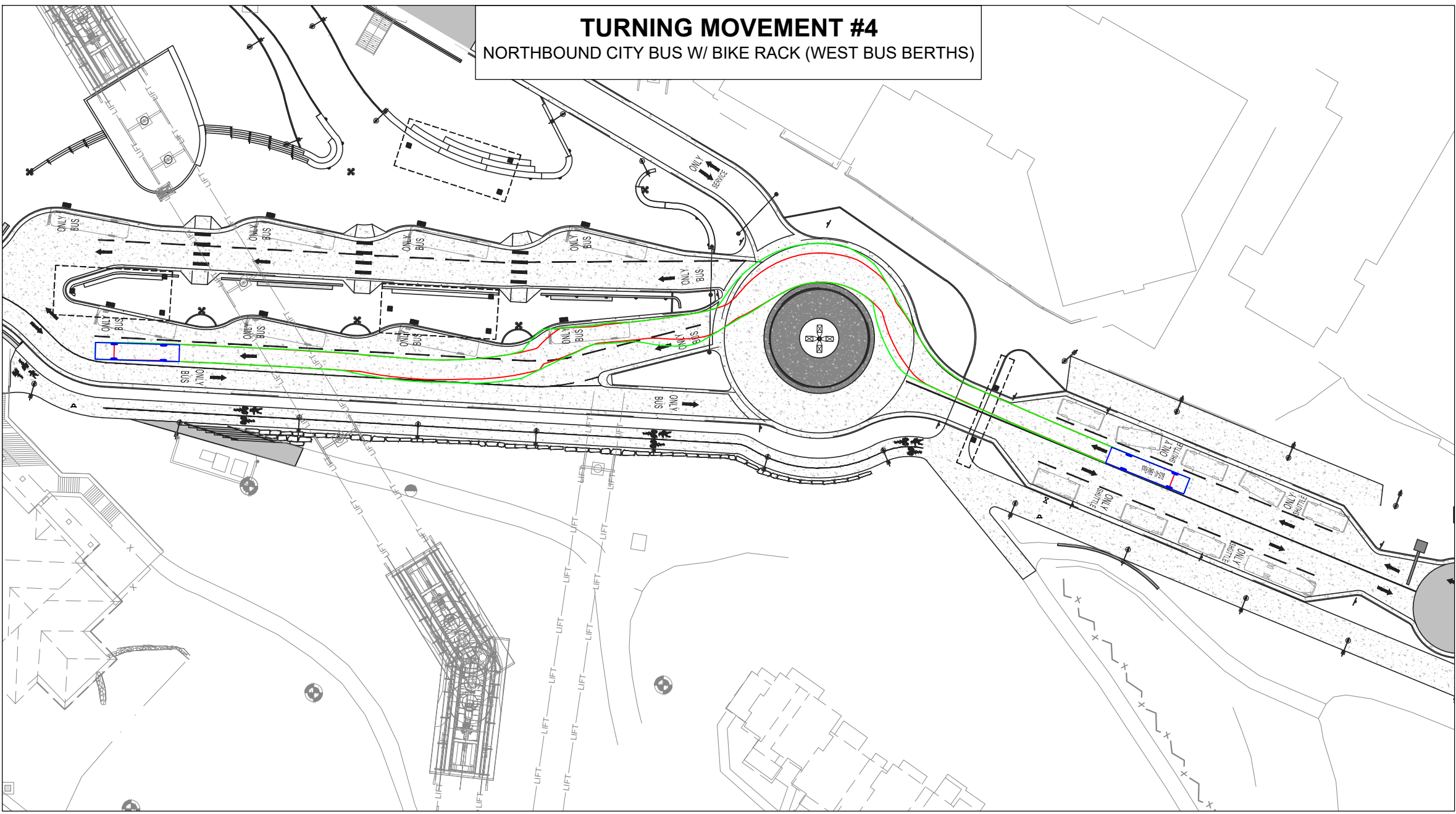
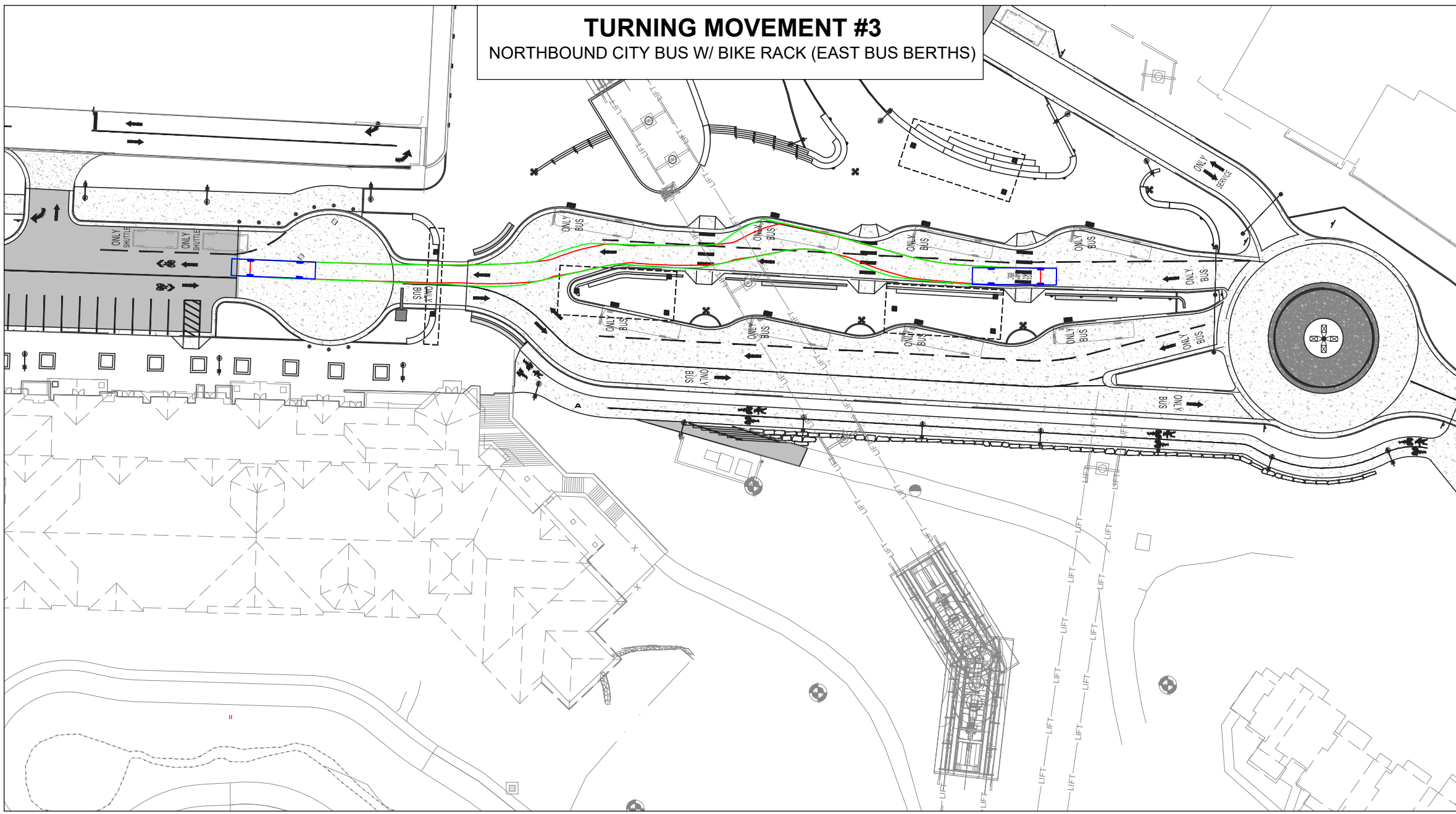
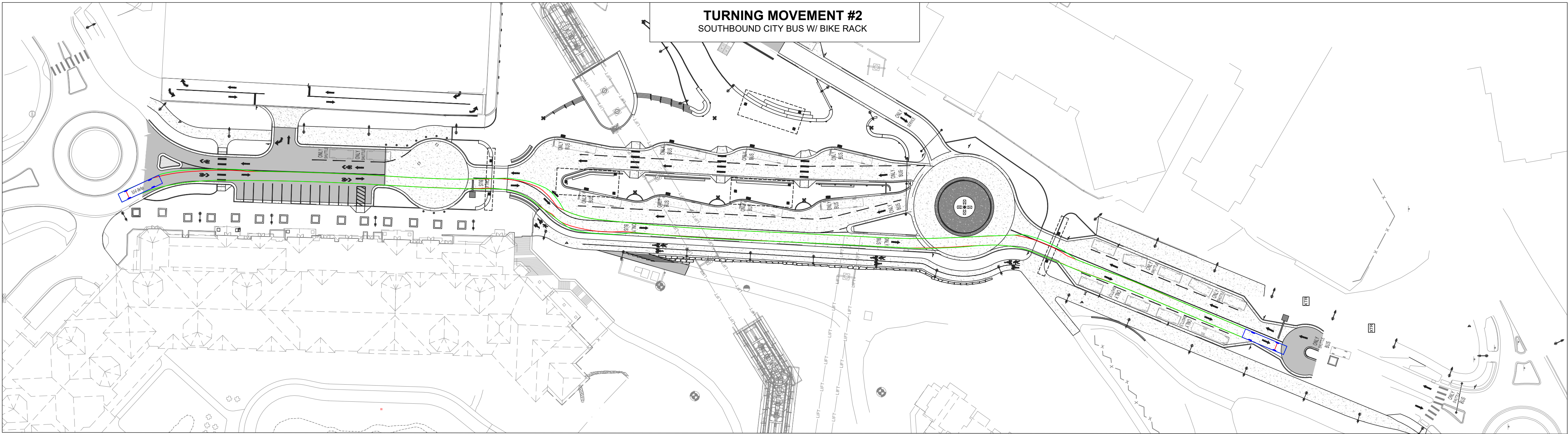
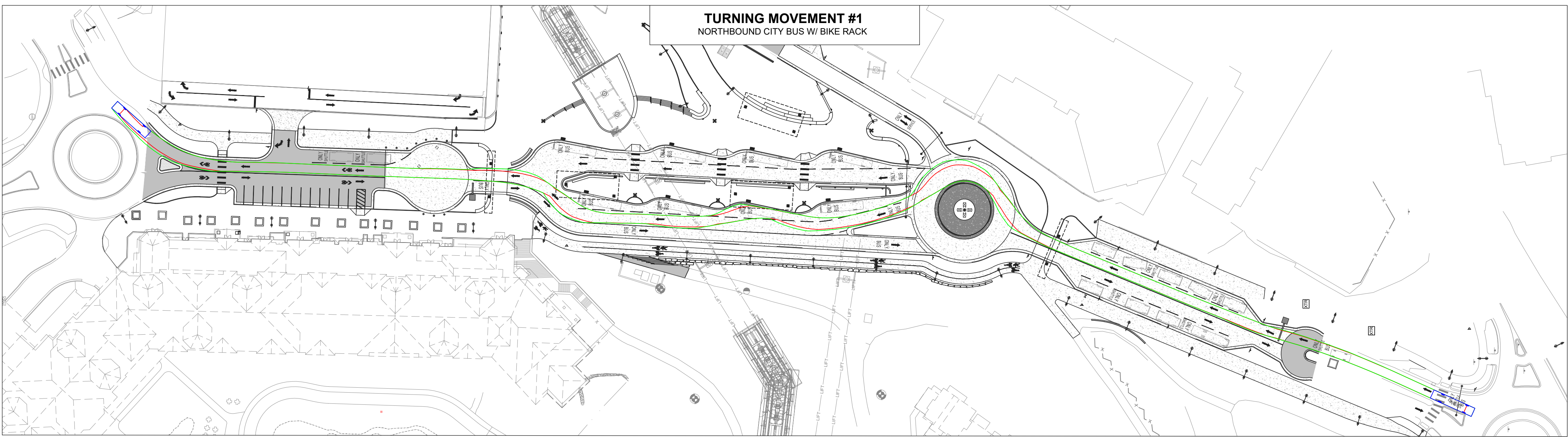
PROJECT NUMBER: 22077/1012-057
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SCHEMATIC
DESIGN

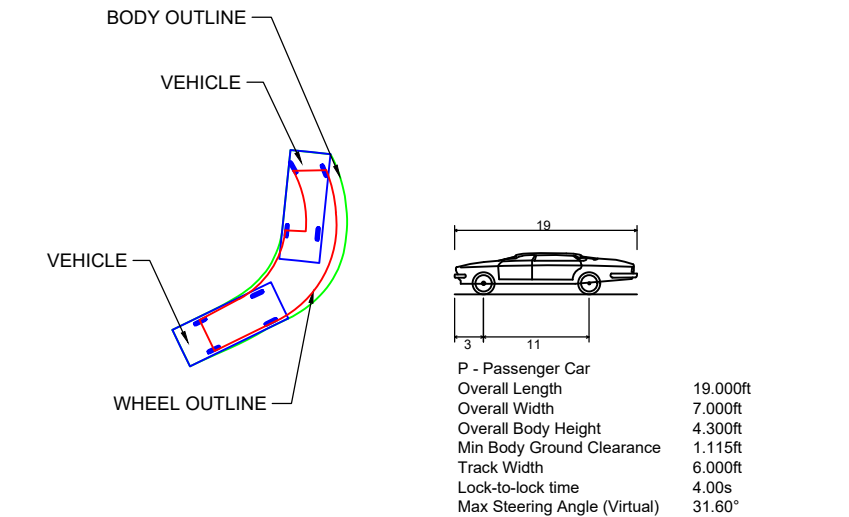
TEMPORARY-PERMANENT
EASEMENT PLAN

SHEET:

C.417



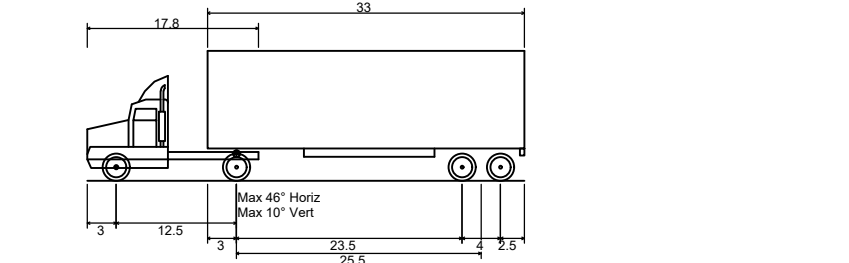
SWEPT PATH ANALYSIS LEGEND:



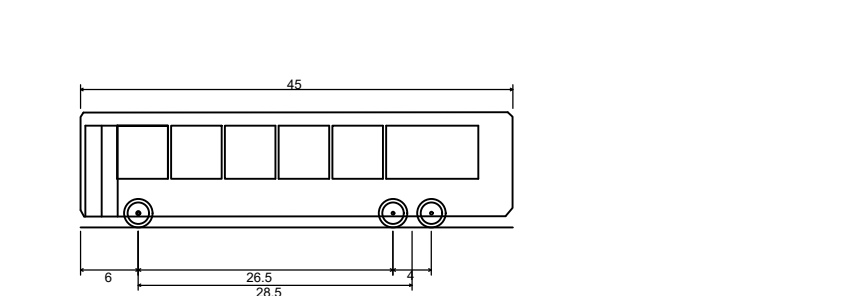
BUS-40 - with Bike Rack	CITY-BUS - City Transit Bus	40.000ft
Overall Length	42.50ft	Overall Length
Overall Width	8.50ft	Overall Width
Overall Body Height	12.0ft	Overall Body Height
Min Body Ground Clearance	1.15ft	Min Body Ground Clearance
Track Width	6.50ft	Track Width
Lock-to-lock time	6.00s	Lock-to-lock time
Max Steering Angle (Virtual)	41.40°	Max Steering Angle (Virtual)



WB-42 - Interstate Semi-Trailer	69.000ft
Overall Length	Overall Length
Overall Width	Overall Width
Overall Body Height	Overall Body Height
Min Body Ground Clearance	Min Body Ground Clearance
Track Width	Track Width
Lock-to-lock time	Lock-to-lock time
Max Steering Angle (Virtual)	Max Steering Angle (Virtual)



WB-40 - Intermediate Semi-Trailer	45.699ft
Overall Length	Overall Length
Overall Width	Overall Width
Overall Body Height	Overall Body Height
Min Body Ground Clearance	Min Body Ground Clearance
Track Width	Track Width
Lock-to-lock time	Lock-to-lock time
Max Steering Angle (Virtual)	Max Steering Angle (Virtual)



Motor Coach - BUS-45 (Regional Bus)	45.0ft
Overall Length	Overall Length
Overall Width	Overall Width
Overall Body Height	Overall Body Height
Min Body Ground Clearance	Min Body Ground Clearance
Track Width	Track Width
Lock-to-lock time	Lock-to-lock time
Curb to Curb Turning Radius	Curb to Curb Turning Radius

NOTES:

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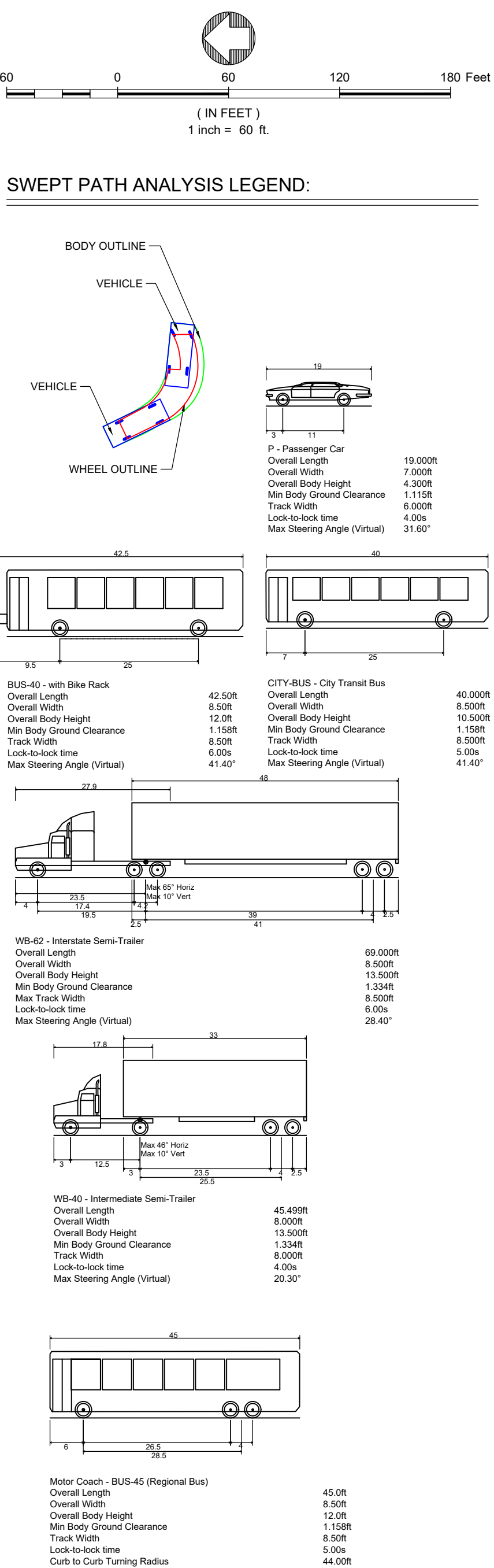
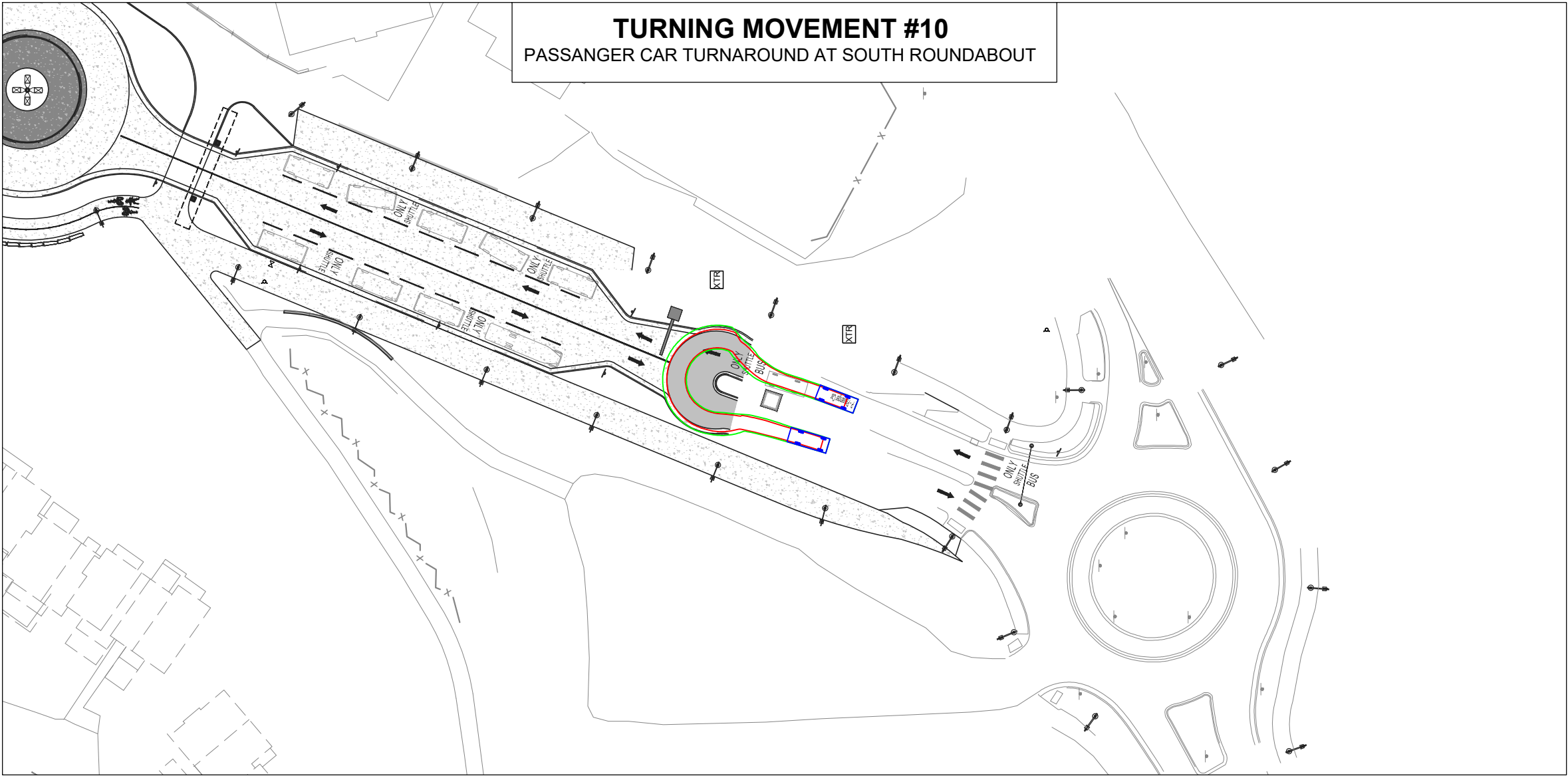
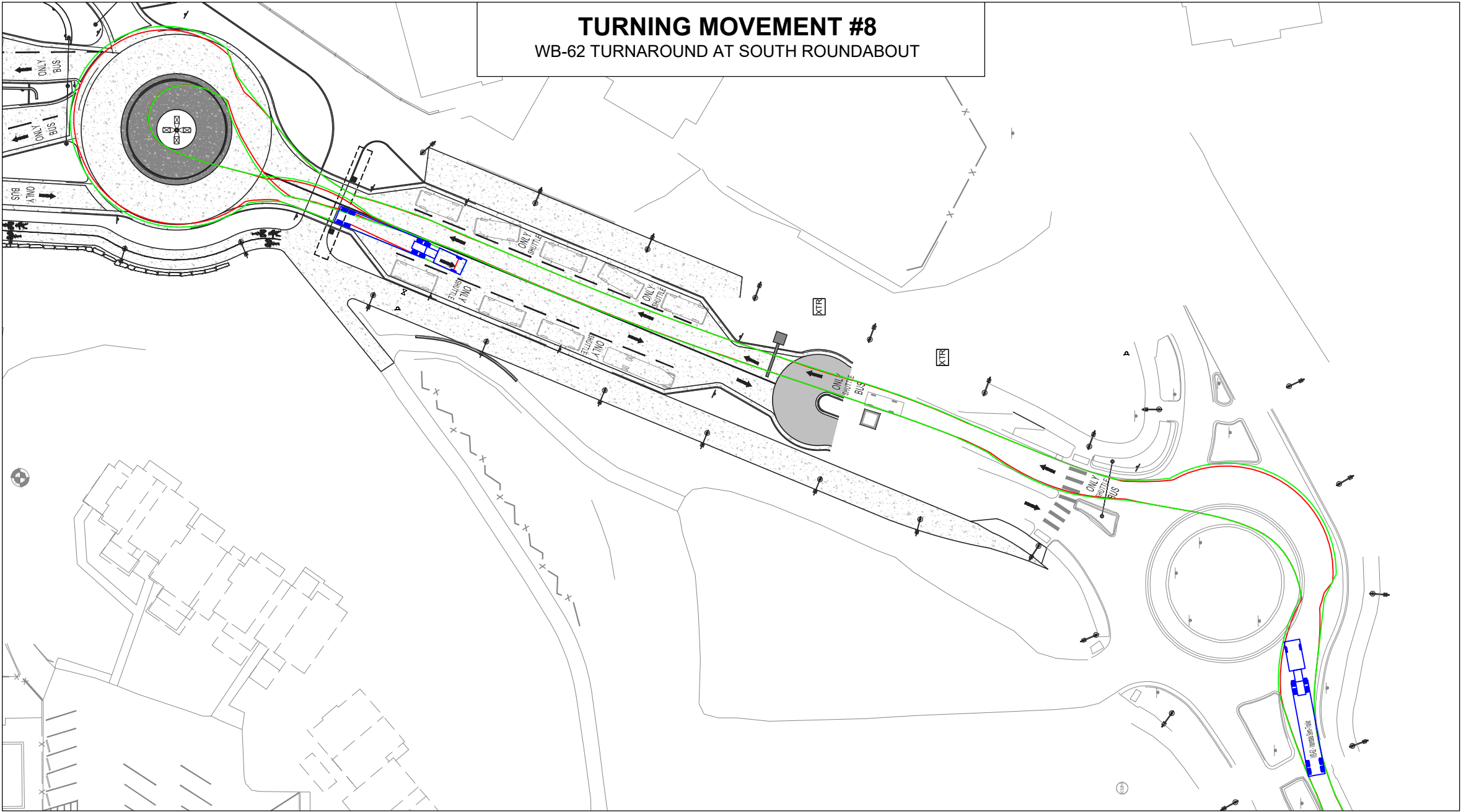
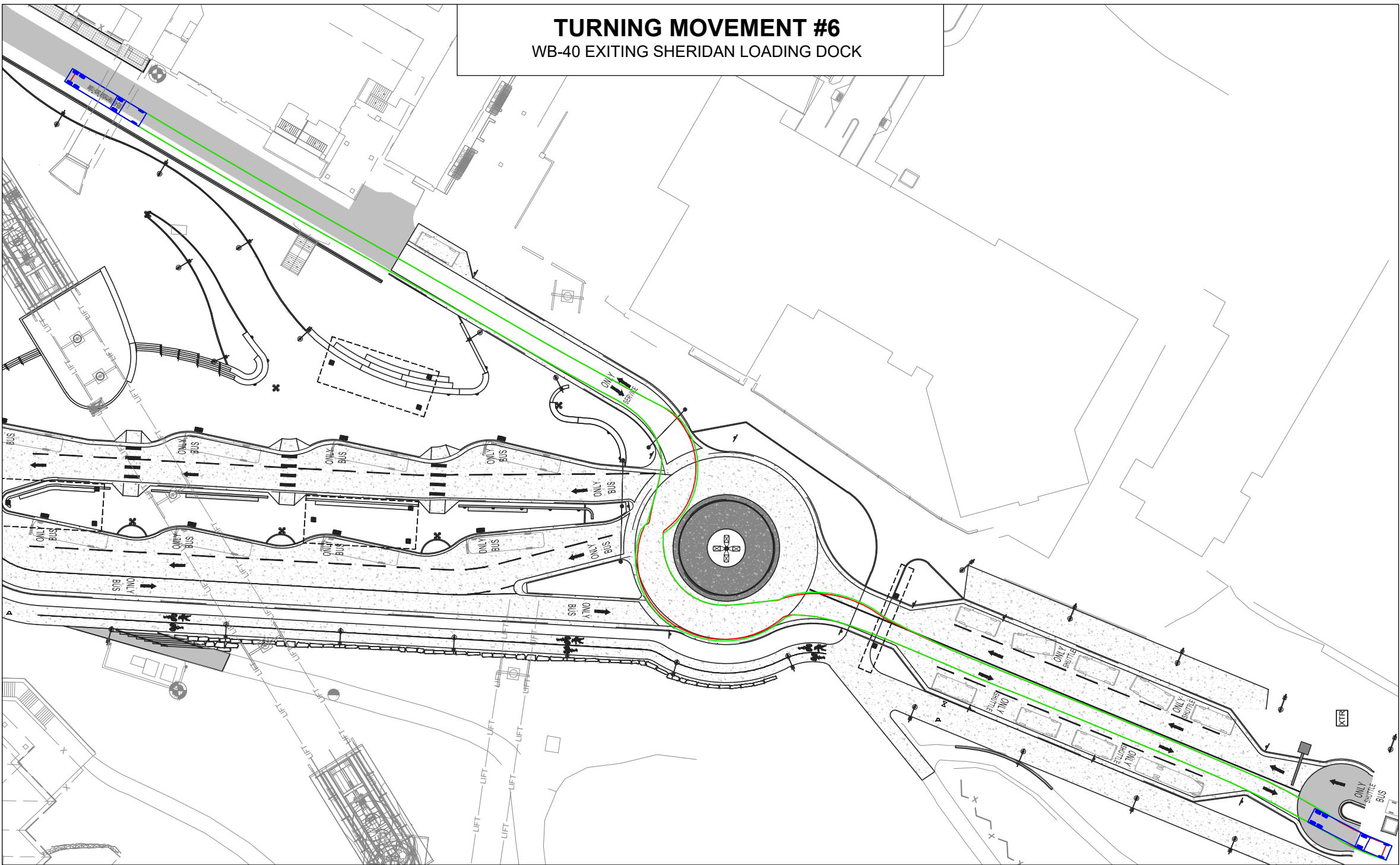
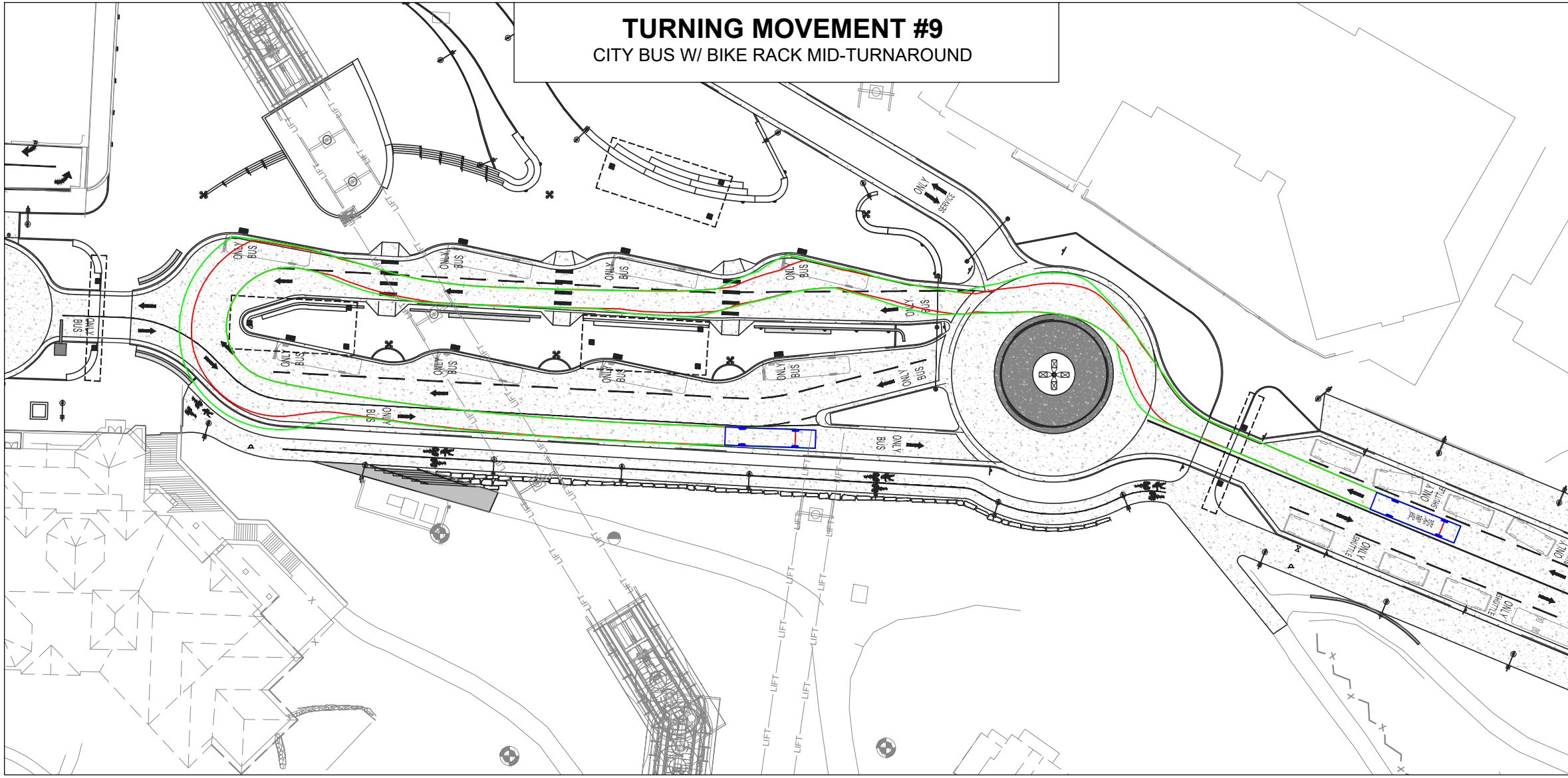
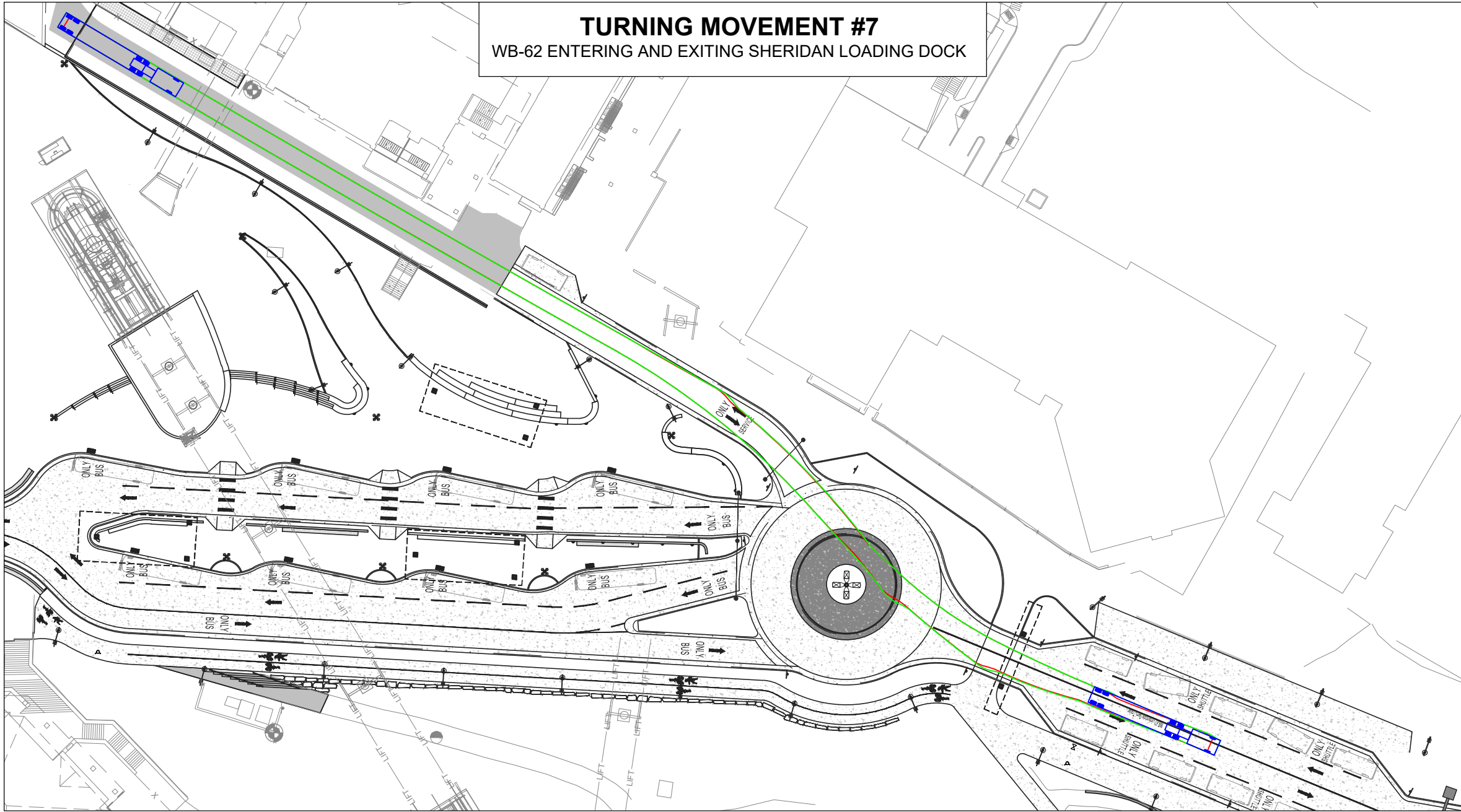
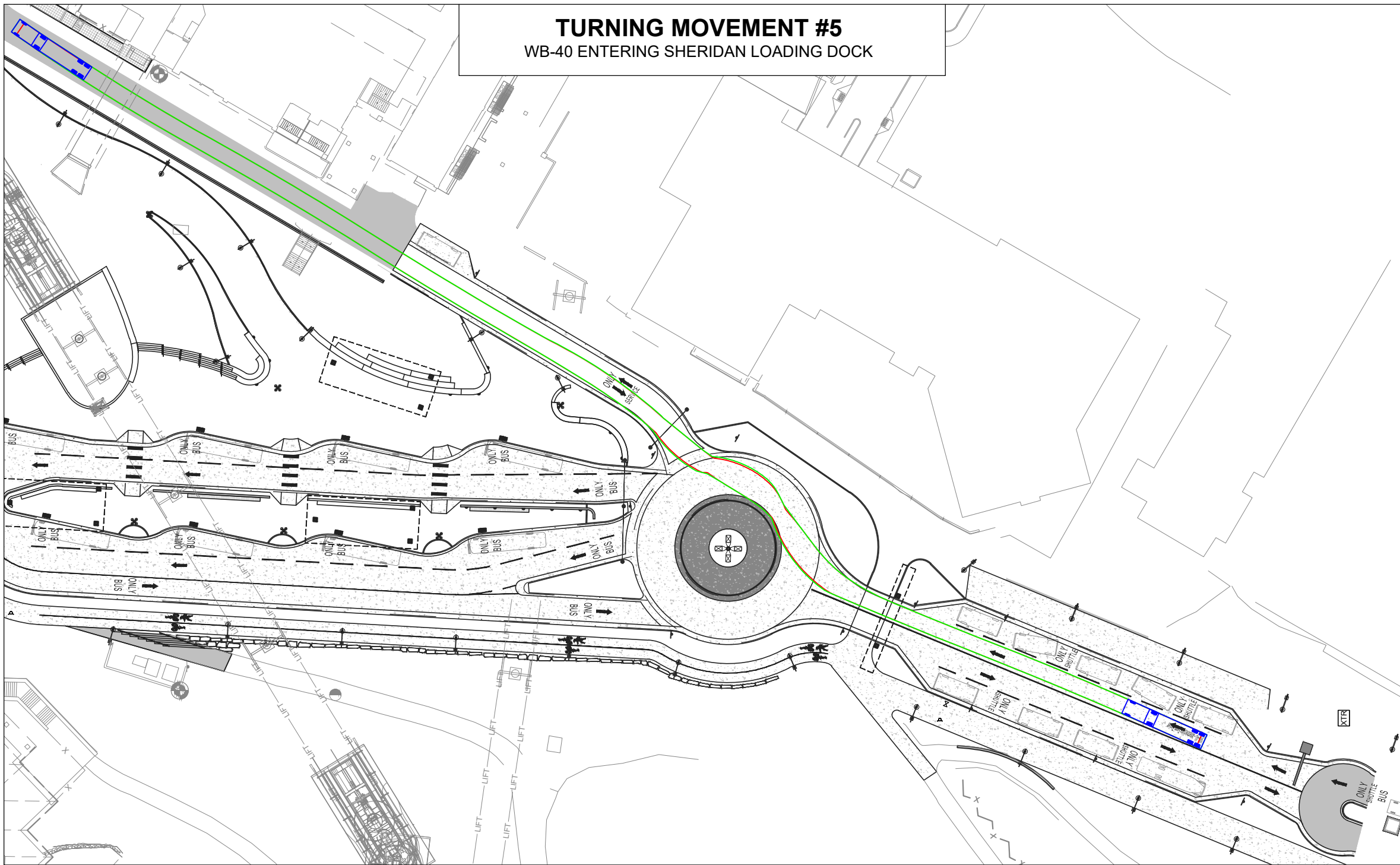
PROJECT NUMBER: 22077/1012-057
DATE: NOVEMBER 03, 2023

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DESIGN

SWEPT PATH EXHIBIT

SHEET:

C.710



NOTES:

1. THE SWEPT PATH ANALYSIS USES THE "CITY-BUS" PER THE STEAMBOAT SPRINGS FIRE PREVENTION SERVICES ADMINISTRATIVE POLICY & PROCEDURE MANUAL, SECTION 4, POLICY NO. 1258-10, DATED SEPTEMBER 11, 2006.

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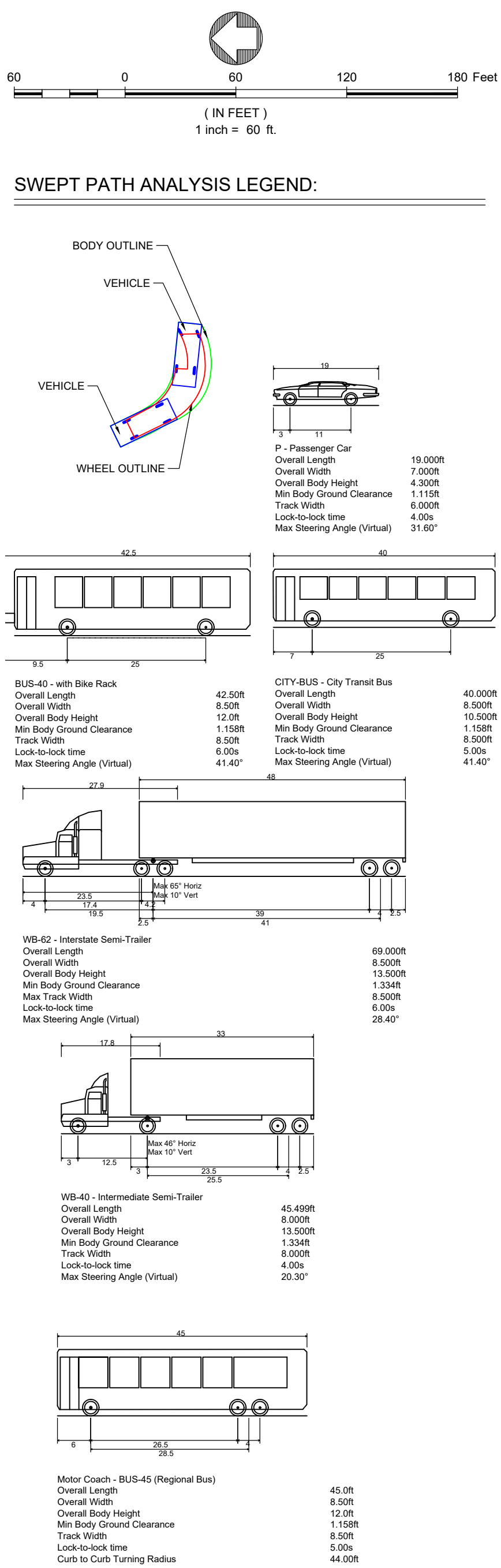
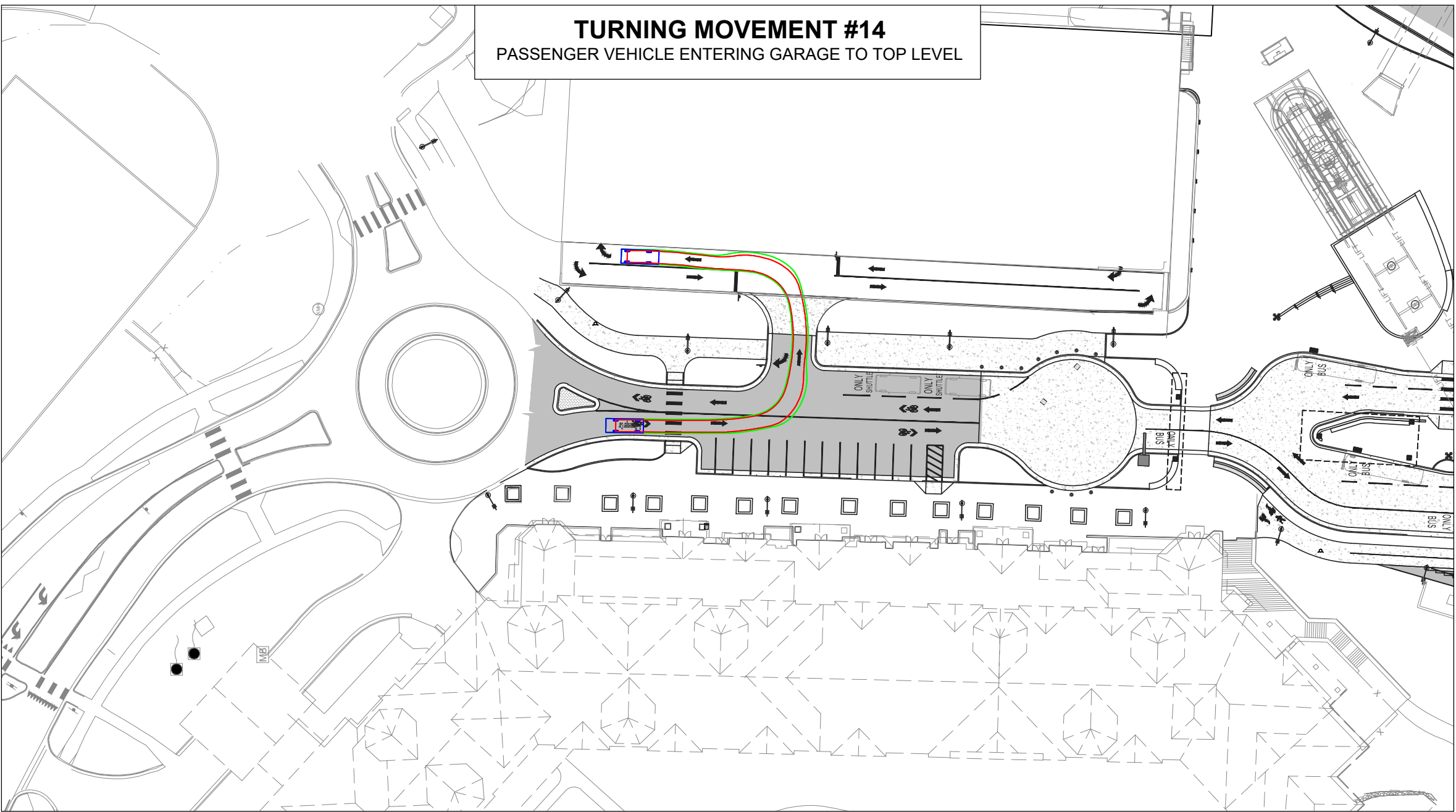
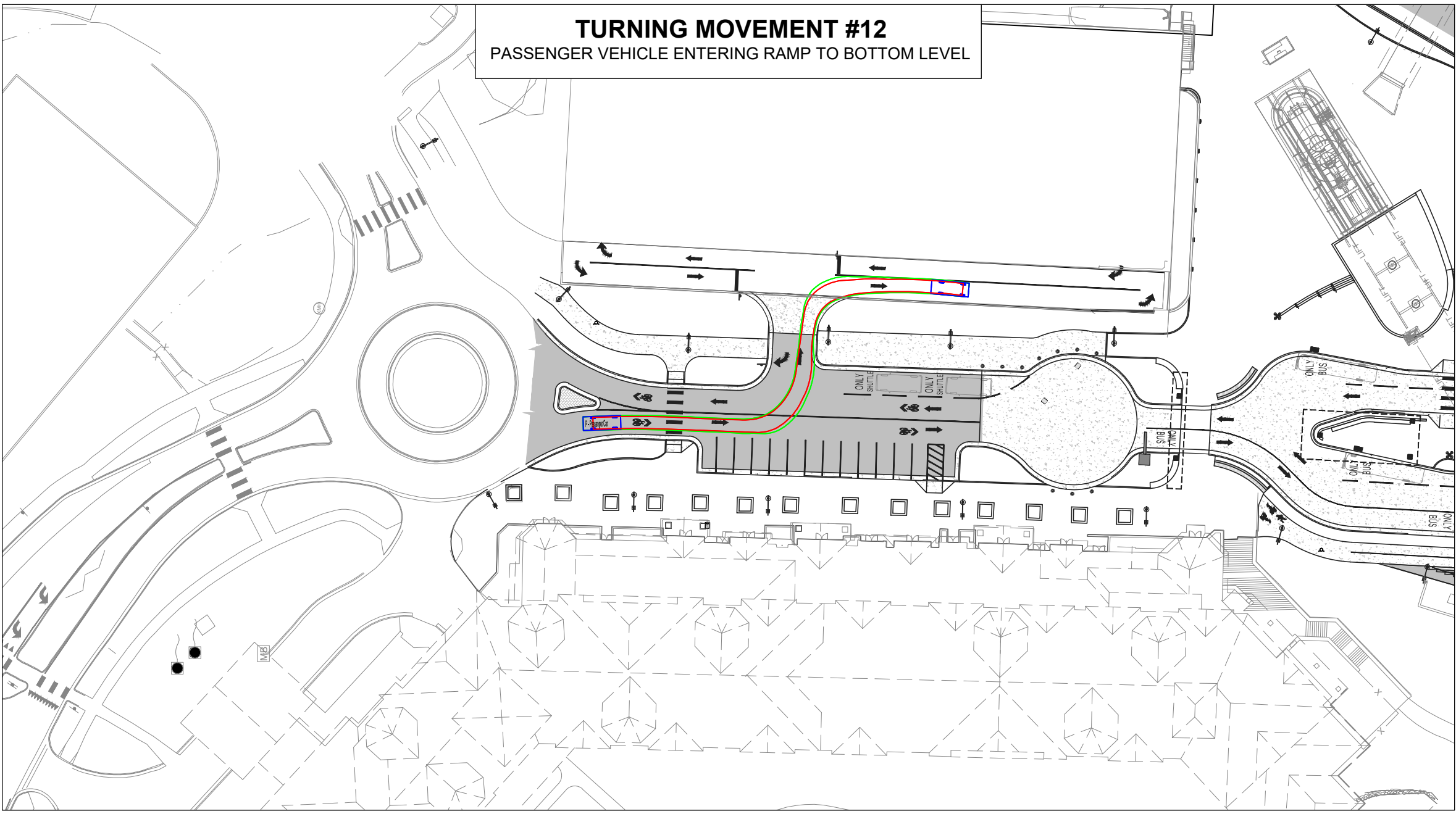
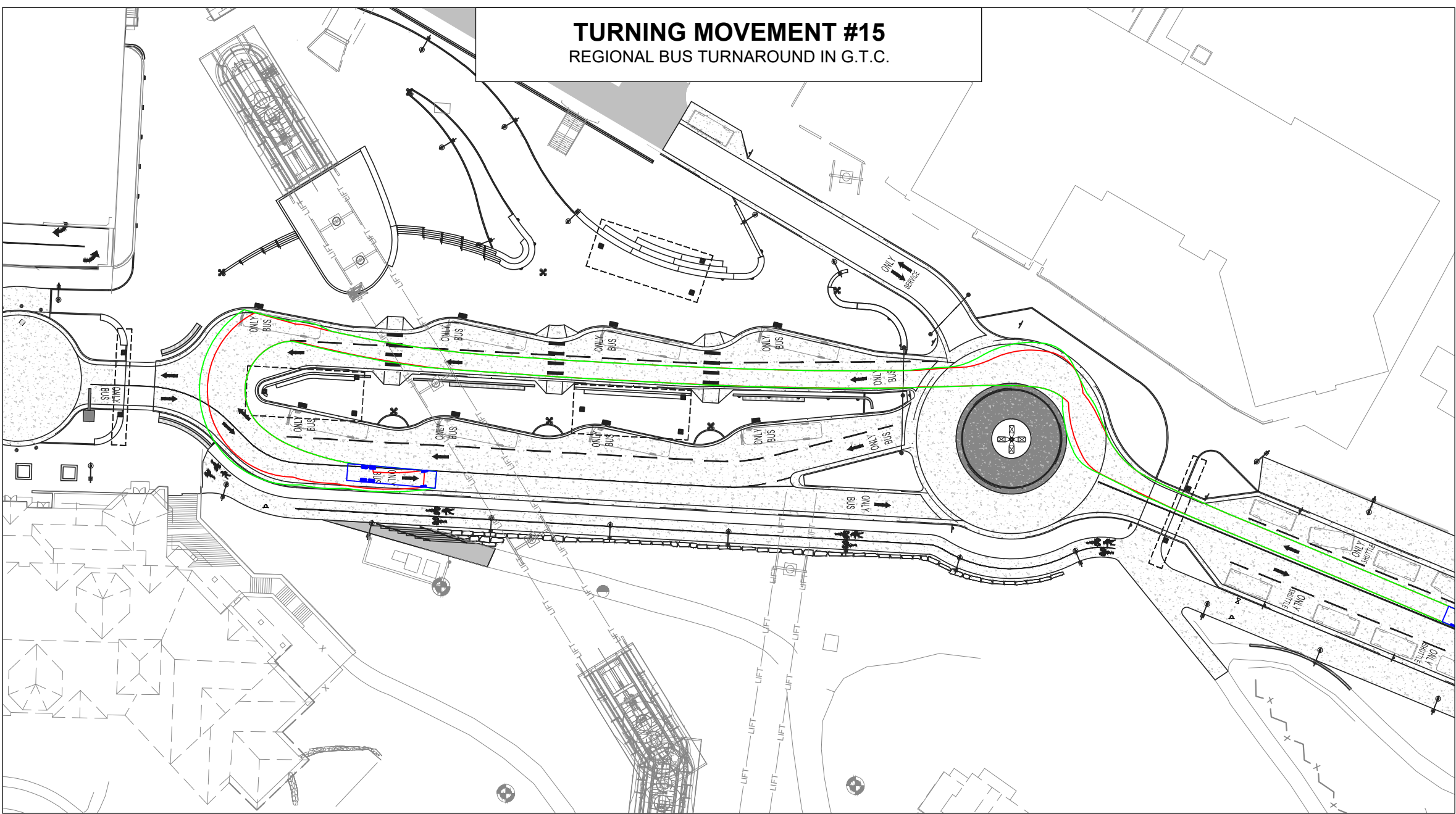
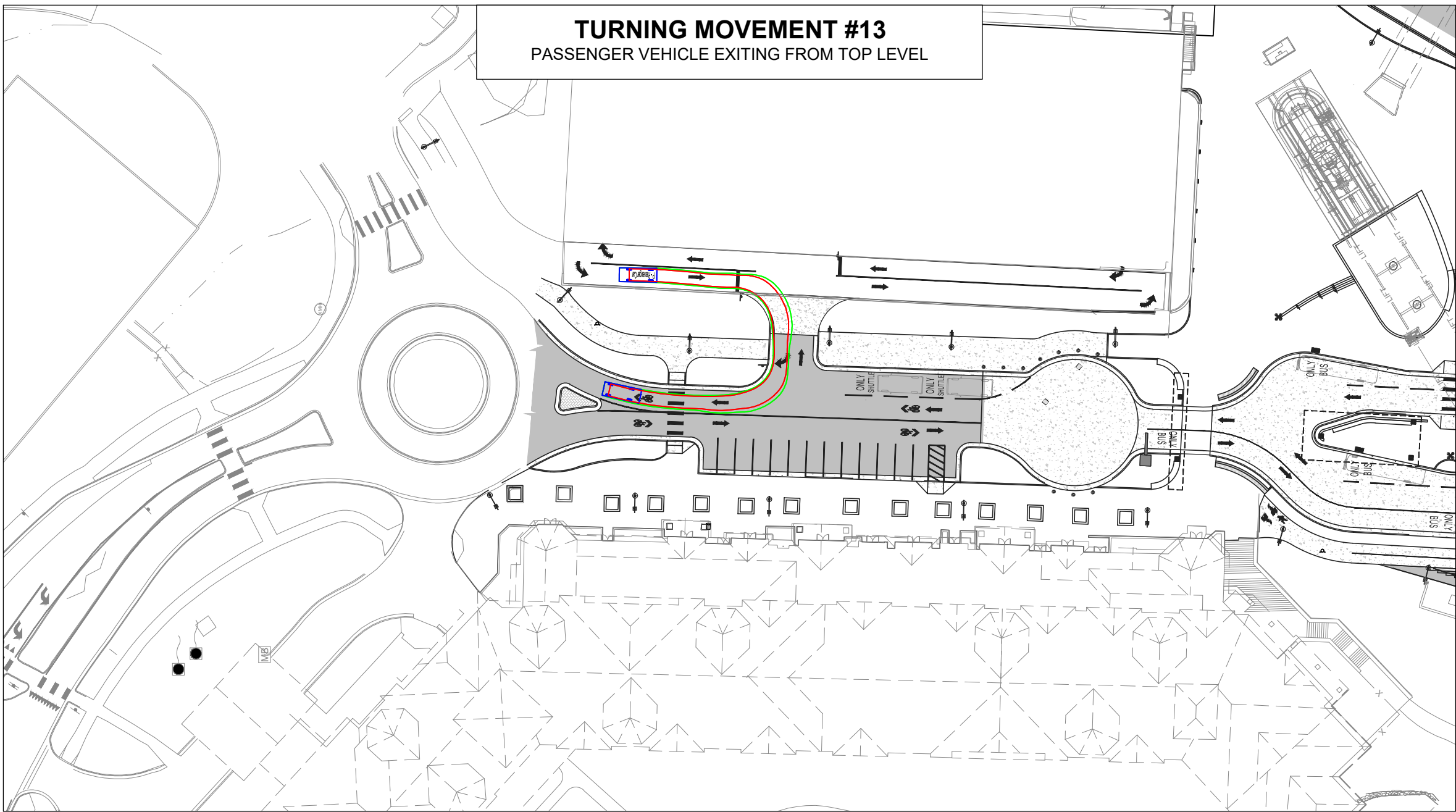
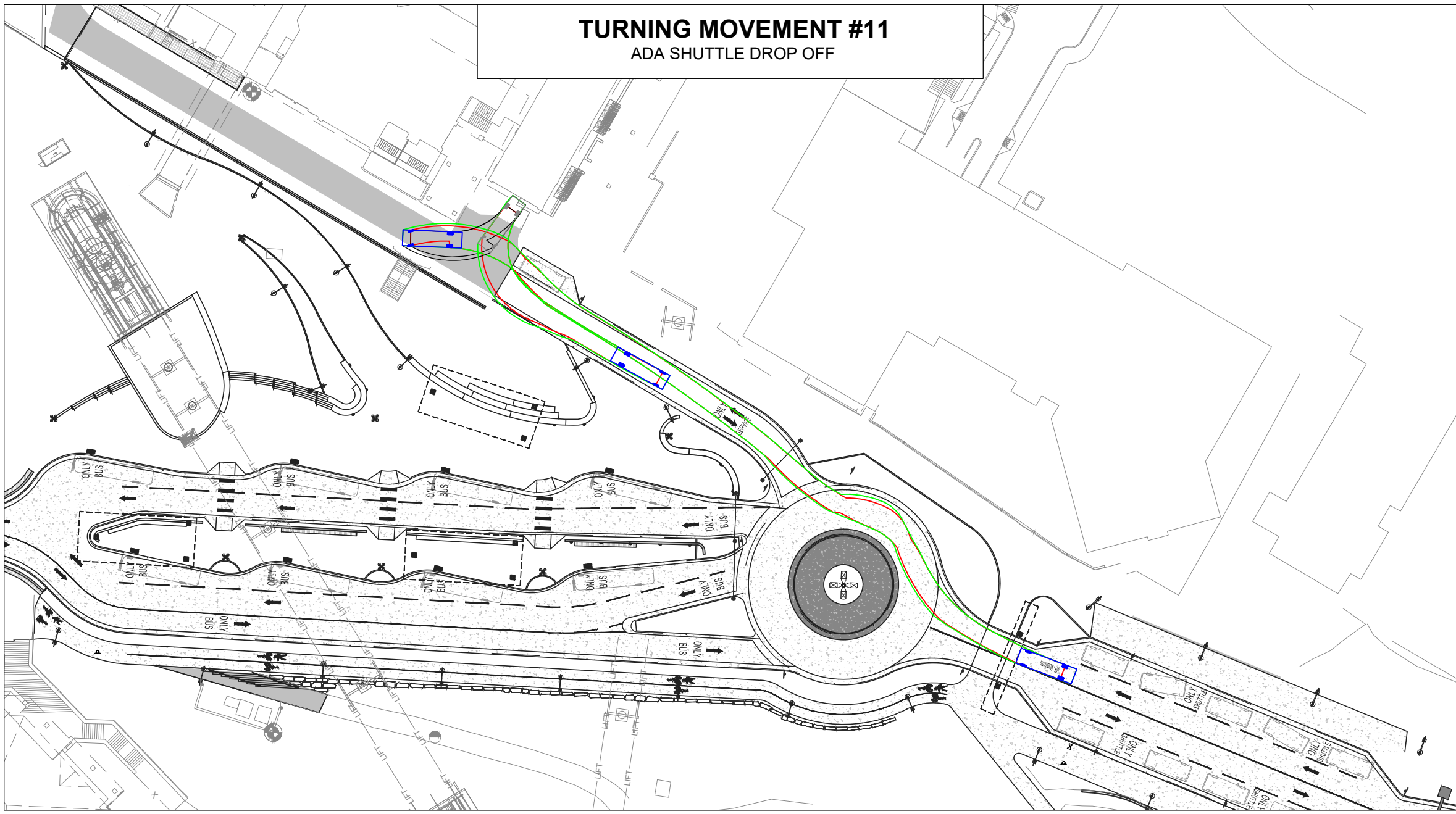
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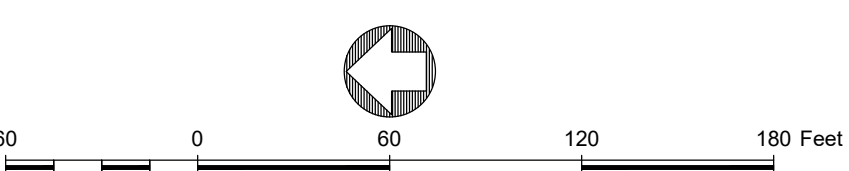
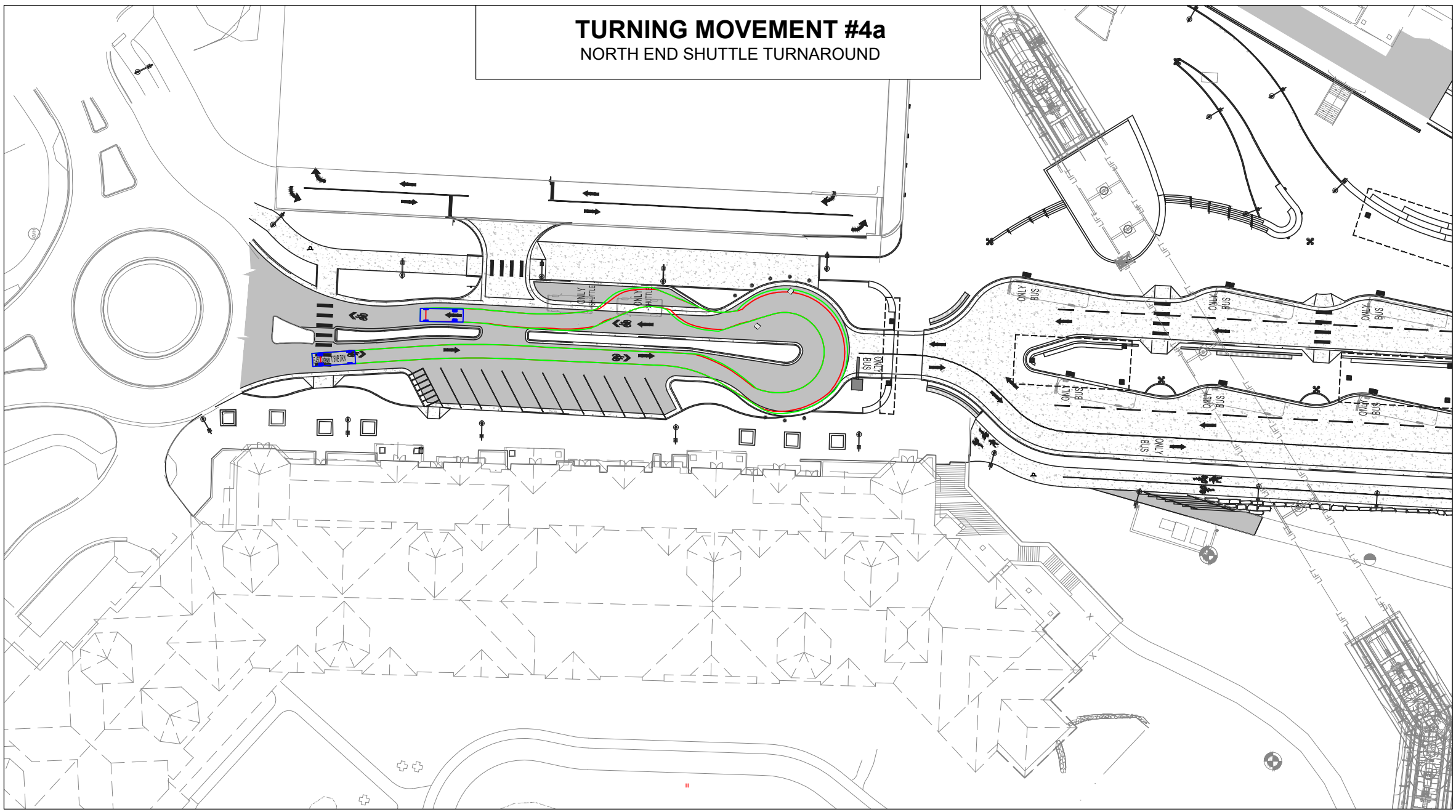
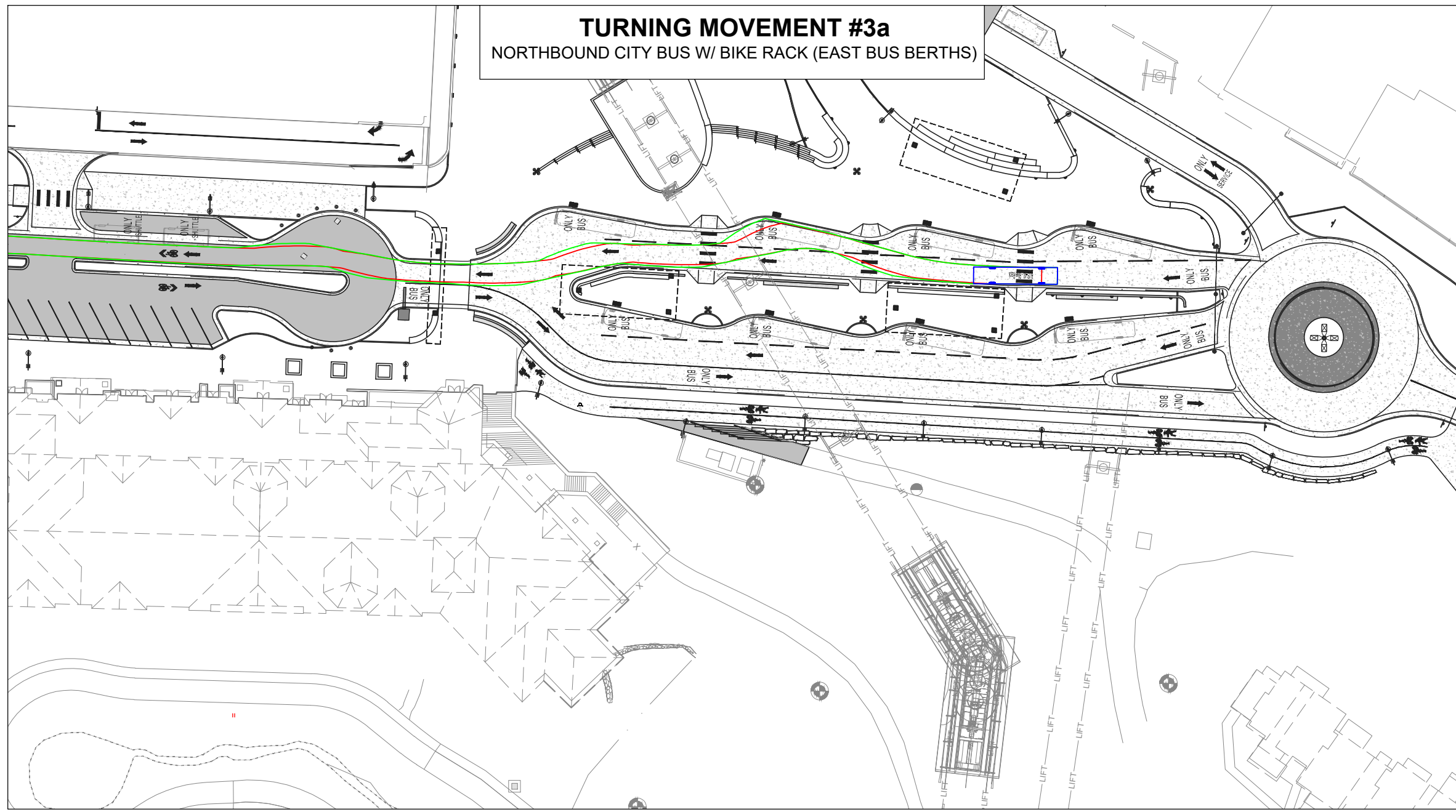
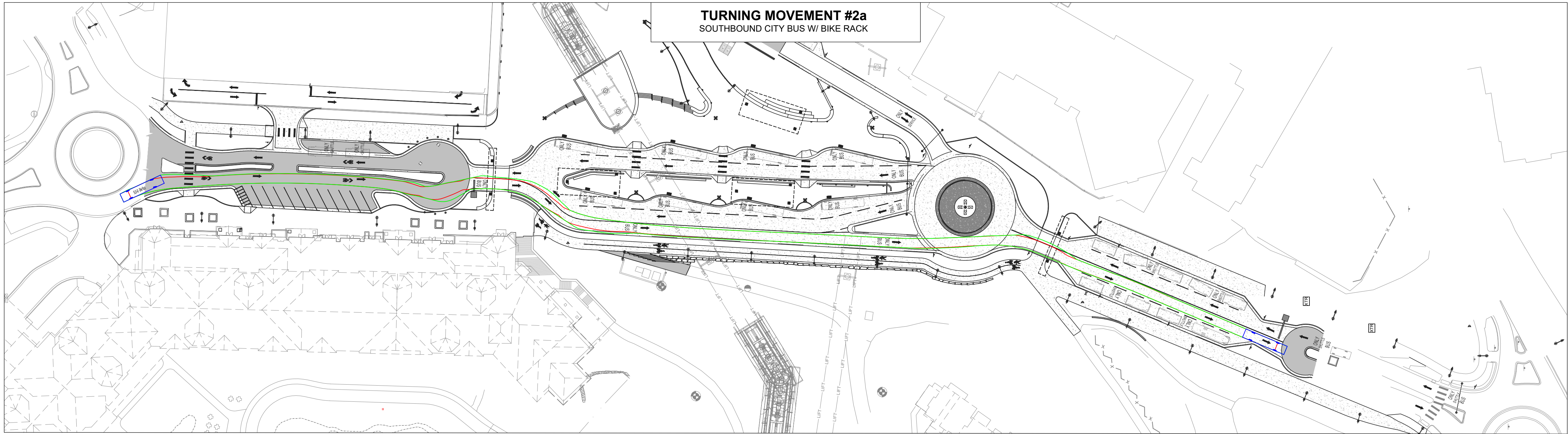
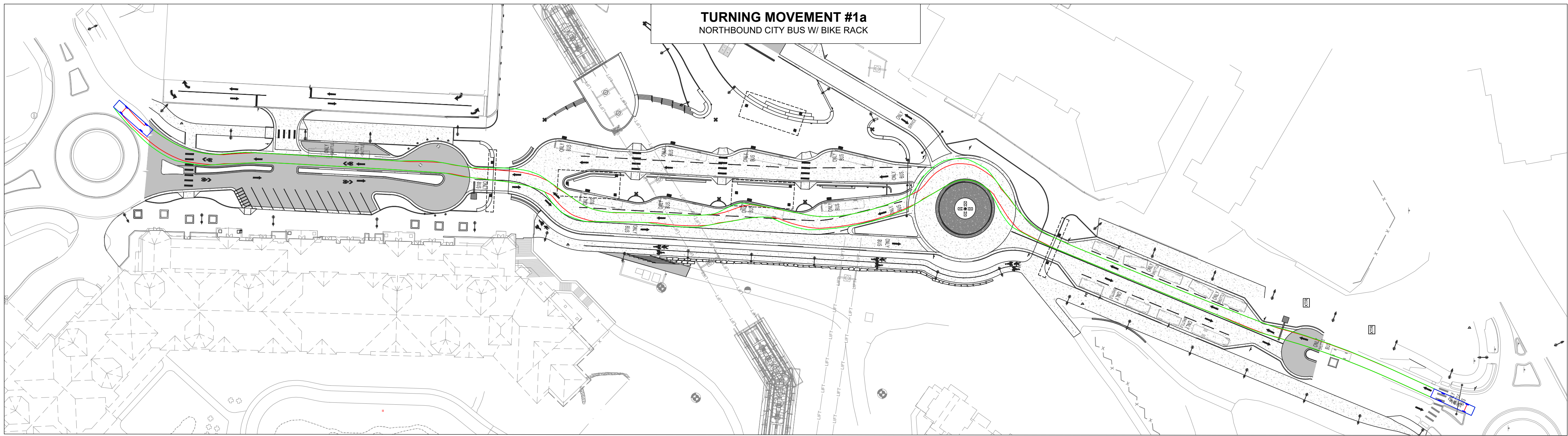
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DESIGN

SWEPT PATH EXHIBIT

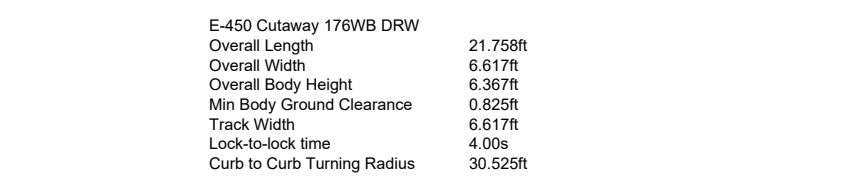
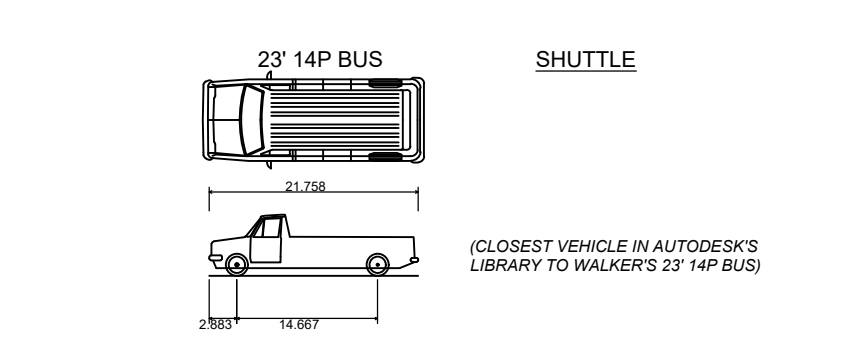
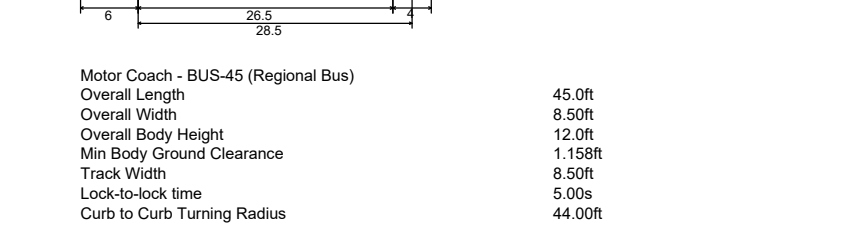
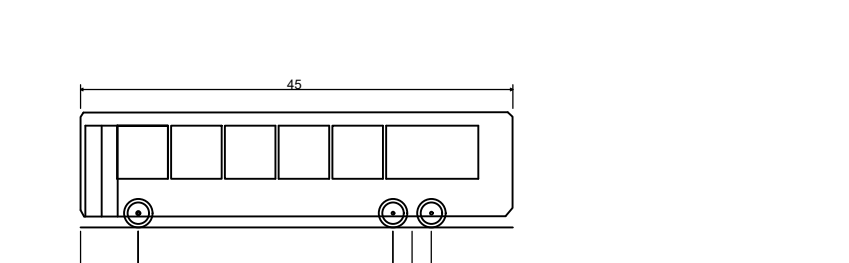
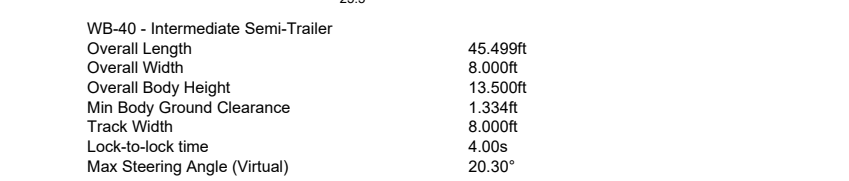
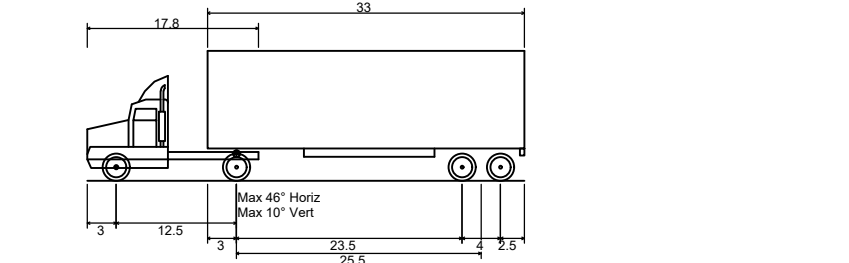
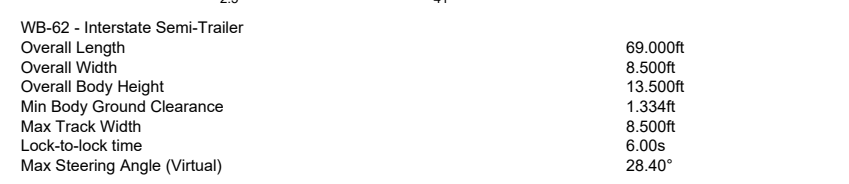
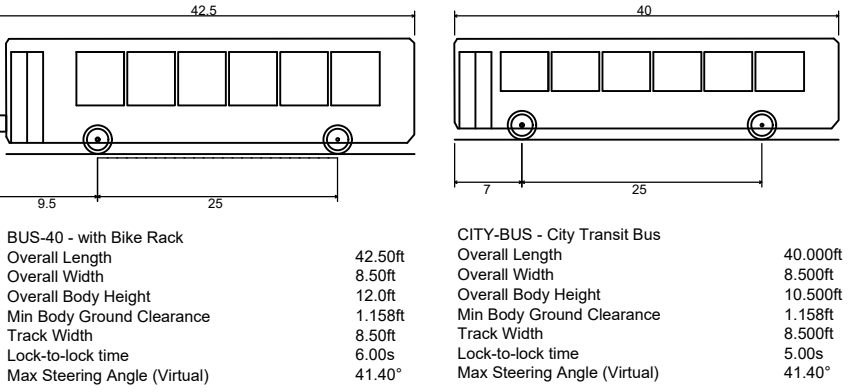
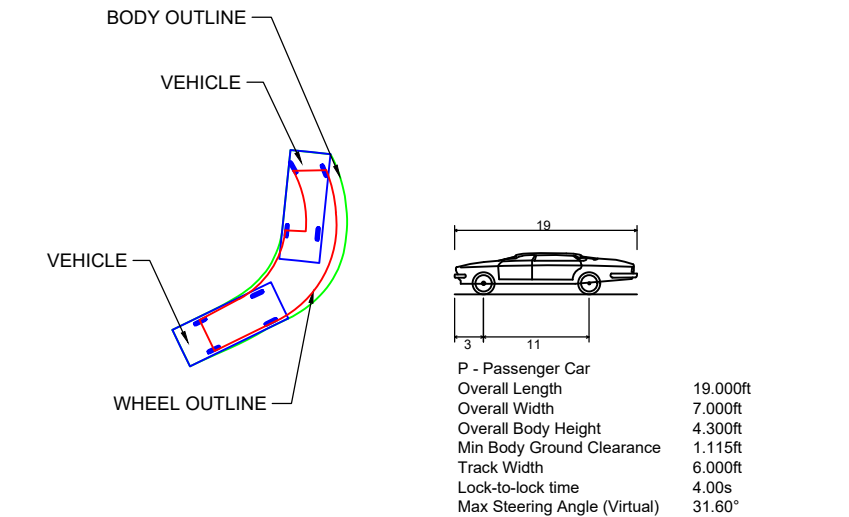
SHEET:

C.711





SWEPT PATH ANALYSIS LEGEND:



NOTES:

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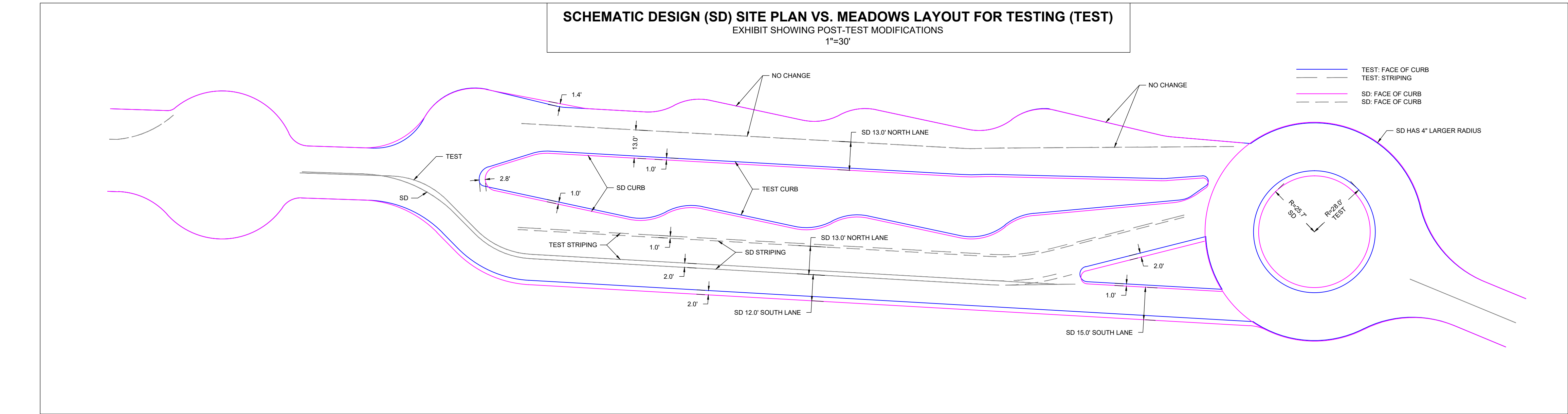
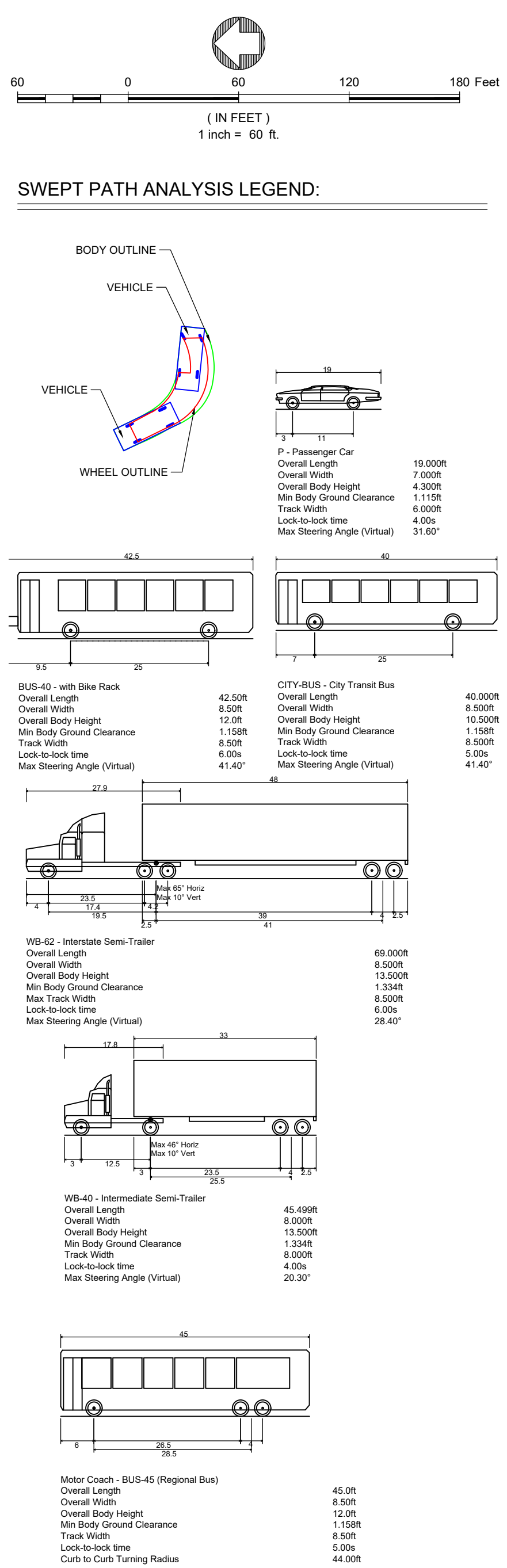
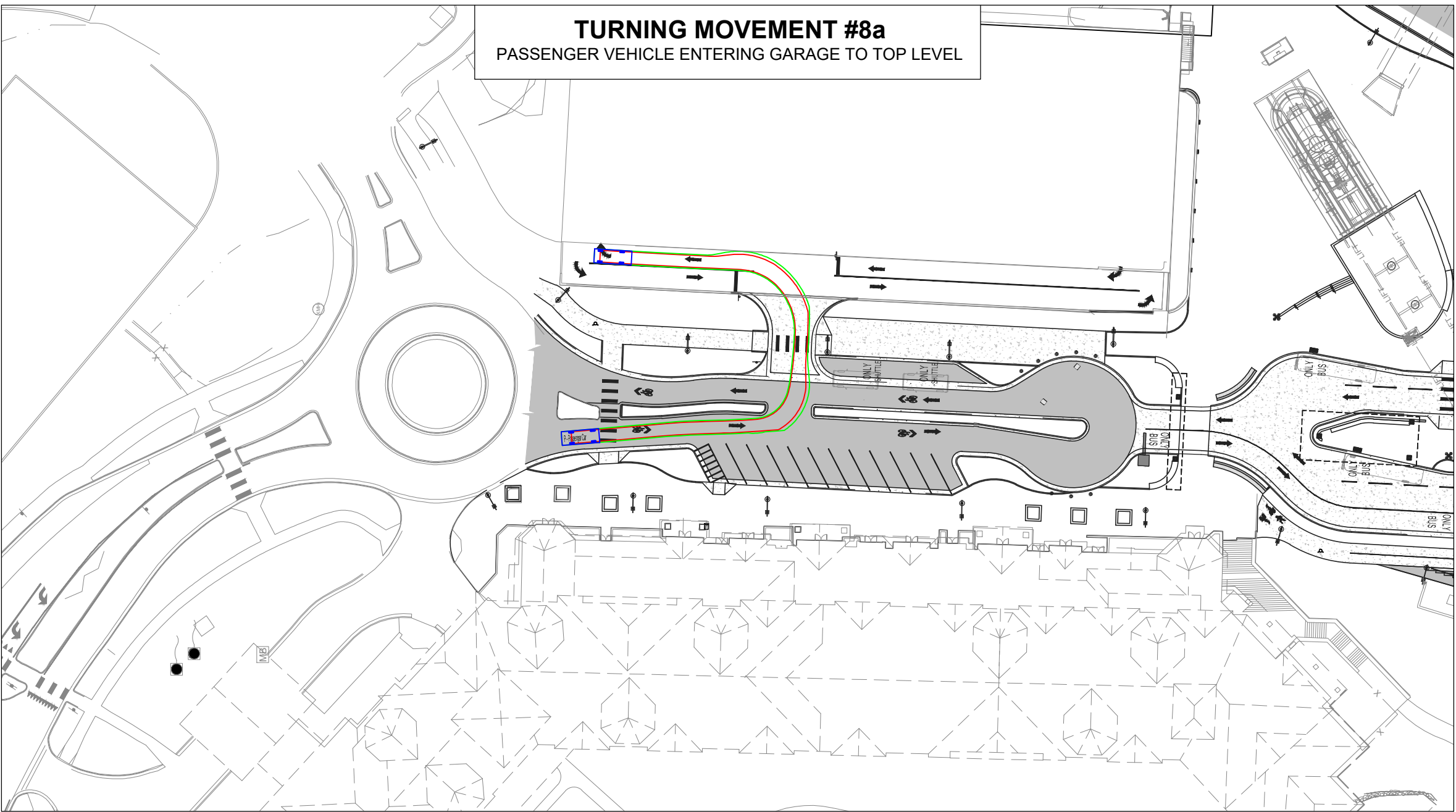
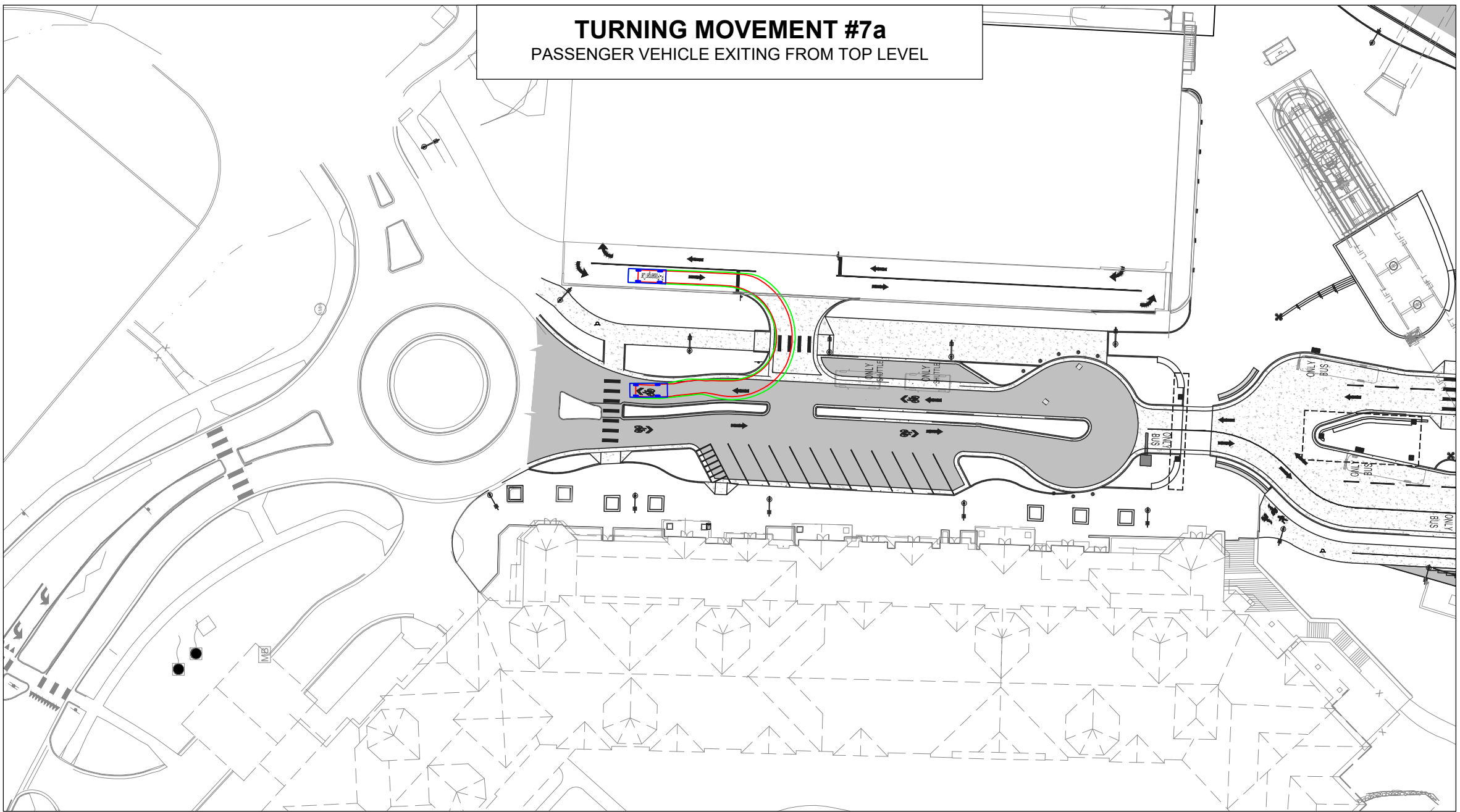
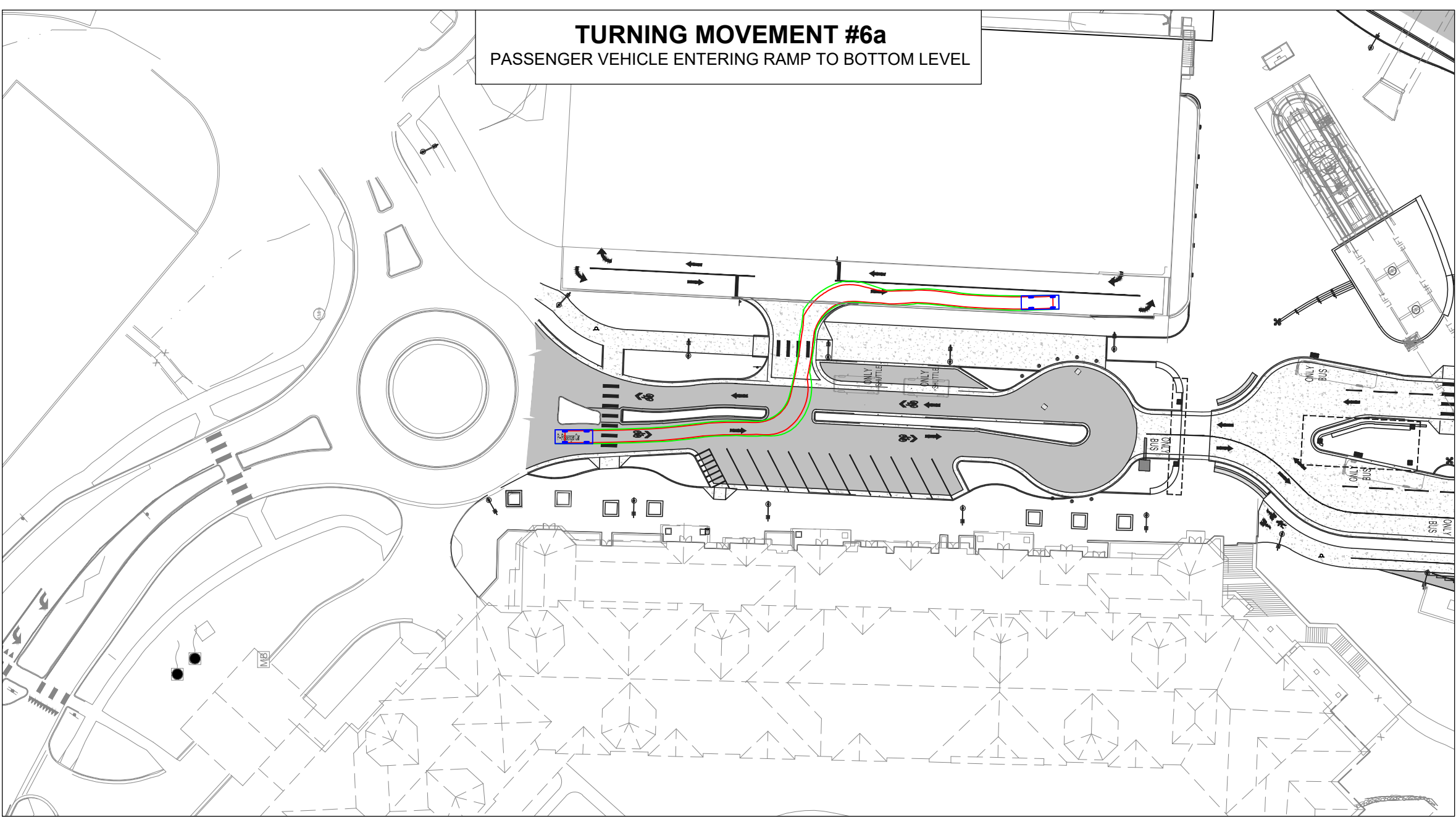
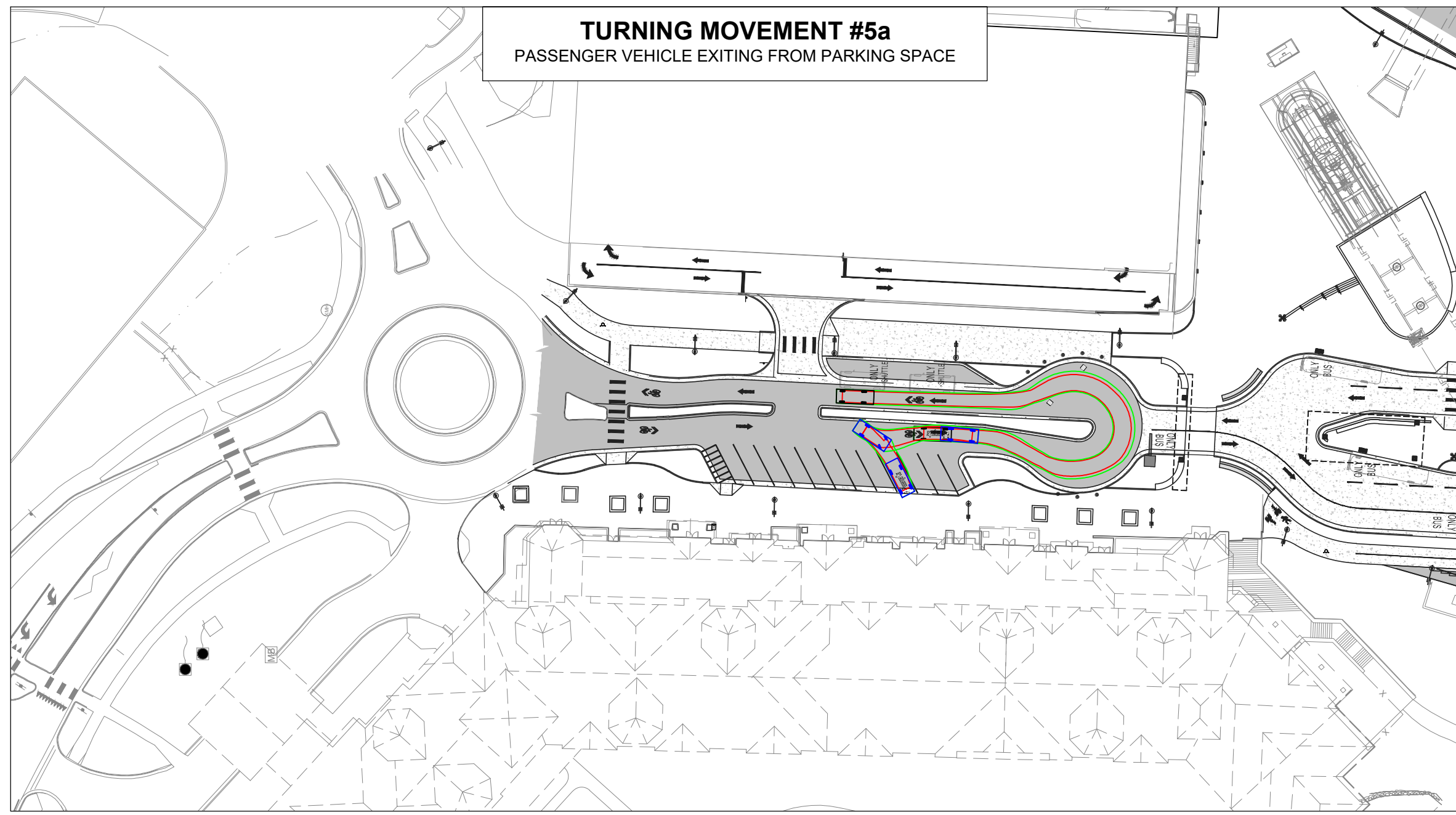
PROJECT NUMBER: 22077/1012-057
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DESIGN

SWEPT PATH EXHIBIT (ALT)

SHEET:

C.713



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SWEPT PATH EXHIBIT (ALT)
& SITE PLAN COMPARISON

SHEET:

C.714