

1.0 PAVING

- 1.1 CONCRETE PAVING
- 1.2 DUMPS/TER PAD (ENCLOSURE BY OTHER, RE: ARCHITECTURAL PLANS)
- 1.3 ASPHALT PAVING
 - 1.3A HEAVY TRAFFIC (MIN. 4" HSA THICKNESS)
 - 1.3B LIGHT TRAFFIC (MIN. 3" HSA THICKNESS)
 - 1.3C REHABILITATION-TBD*

* APPROXIMATE AREAS SUBJECT TO MILL/OVERLAY, PATCHING, REPAIR - ACTUAL LIMITS TO BE FIELD DETERMINED AND COORDINATED WITH GEOTECH

1.4 CONCRETE CURB & GUTTER

- 1.4A CATCH CURB (VC)
- 1.4B SPILL CURB (SC)
- 1.4C VALLEY PAN (VP)
- 1.4D THICKENED EDGE RIBBON CURB
- 1.4E TAPERED CURB (T)
- 1.4F BARRIER (T1 T2)









1.5 CURB RAMP

- 1.6 DETECTABLE WARNING SURFACE
- 1.7 PAVERS (BY OTHERS)
- 1.8 GRAVEL SURFACING
- 1.9 CRUSHER FINE

1.0 PAINTING / STRIPING

- 2.1 STOP BAR
- 2.2 4 INCH WIDE SOLID WHITE (PARKING/FOG LINE)
- 2.3 DOUBLE YELLOW CENTERLINE
- 2.4 ADA PARKING SYMBOL
- 2.5 ADA LOADING AISLE
- 2.6 CROSSWALK STRIPING
- 2.7 BIKE LANE SYMBOL
- 2.8 SHARED LANE SYMBOL
- 2.9 TURN ARROW

- 3.1 ADA PARKING SIGNAGE
- 3.2 FIRELANE NO PARKING SIGN
- 3.3 AS SHOWN ON PLANS
- 3.4 STOP SIGN
- 3.5 DO NOT ENTER SIGN
- 3.6 YIELD SIGN
- 3.7 RIGHT TURN ONLY SIGN
- 3.8 TURN ARROW
- 3.9 BUS STOP (RE: CITY OF STEAMBOAT SPRINGS)

EX. ASPHALT	
PR. ASPHALT	
±PR. ASPHALT REHABILITATION*	
EX. CONCRETE	
PR. CONCRETE	
PR. PAVERS	
PR. GRAVEL SHOULDERING	
PROPOSED SIGN	

1. ALL SIGNAGE AND MARKINGS SHALL CONFORM TO THE CURRENT VERSION OF THE M.U.T.C.D.
2. ALL SYMBOLS, INCLUDING ARROWS, ONLY: CROSSWALKS, STOP BARS, ETC. SHALL BE PRE-FORMED THERMOPLASTIC.
3. ALL LANE LINES FOR ASPHALT PAVEMENT SHALL RECEIVE TWO COATS OF LATEX PAINT WITH GLASS BEADS.
4. ALL LANE LINES FOR CONCRETE SHALL BE EPOXY PAINT.
5. EPOXY APPLICATIONS SHALL BE APPLIED AS SPECIFIED IN CDOT STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION.
6. ALL SURFACES SHALL BE THOROUGHLY CLEANED PRIOR TO INSTALLATION OF STRIPPING OR MARKINGS.
7. THE SIGN INSTALLER SHALL BE RESPONSIBLE FOR LOCATING AND PROTECTING ALL UNDERGROUND UTILITIES.
8. SPECIAL CARE SHALL BE TAKEN IN SIGN LOCATION TO ENSURE AN UNOBSTRUCTED VIEW OF EACH SIGN.
9. SIGNAGE AND STRIPING HAS BEEN DETERMINED BY INFORMATION AVAILABLE AT THE TIME OF REVIEW. ENGINEER RESERVES THE RIGHT TO REQUIRE ADDITIONAL SIGNAGE AND/OR STRIPING IF CONDITIONS WARRANT SUCH SIGNAGE ACCORDING TO THE MUTCD OR THE CDOT MA STANDARDS. ALL SIGNAGE AND STRIPING SHALL FALL UNDER ANY REQUIREMENTS FOR WARRANTY PERIODS FOR NEW CONSTRUCTION (EXCEPT FOR NORMAL WEAR ON TRAFFIC MARKINGS).
10. SLEEVES FOR DELINEATORS AND SIGN POSTS SHALL BE REQUIRED FOR USE IN ISLANDS/MEDIANS.
11. SEE SOILS REPORT FOR PAVEMENT, SUBGRADE AND MATERIAL PREPARATION, DESIGN AND RECOMMENDATIONS.
12. THE SCOPE REFLECTED ON THIS PLAN SHALL ONLY BE USED FOR ITEMS SPECIFIED AND DETAILED ON DRAWINGS PREPARED BY LANDMARK CONSULTANTS, INC. REFERENCES TO "BY OTHERS", "LANDSCAPE", "LANDSCAPE PLANS" AND/OR SIMILAR NOTATION ARE EXCLUDED FROM THIS PLAN. CONTACT LANDMARK CONSULTANTS, INC. AND THE DESIGN TEAM IF DISCREPANCIES ARE DISCOVERED.


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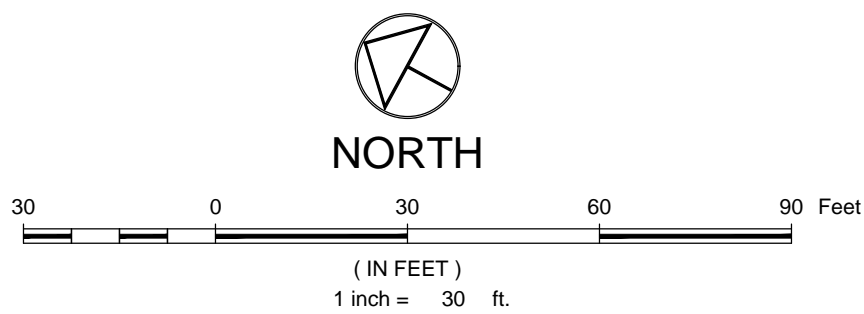
811

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		CIVIL ENGINEERS SURVEYORS 141 9th Street – P.O. Box 774943 Steamboat Springs, Colorado 80477 (970) 871-3494 www.LANDMARK-CO.com			
These drawings are provided by Landmark Consultants, Inc. to be used for any type of construction or contacting a Professional Engineer in the State of Colorado. LANDMARK CONSULTANTS, INC.					
PROJECT:	NO.	DATE:	BY:	DESCRIPTION:	
<p style="margin: 0;">REVIEWED FOR CODE COMPLIANCE</p> <p style="margin: 0; color: red;">02/24/2018</p> <p style="margin: 0;">Steamboat Basecamp - Phase I Materials, Signage & Striping</p>	1	07/13/22	Grip	Saw Service Modification	
	DTE:	5	06/14/22	Grip	All #2 - Additional Spot Elevations, road lateral
	CONTACT:	6	09/27/22	Grip	Additional info for Bus Stop and Headwall
	E MAIL:	7	02/08/23	Grip	Rev courtyard, updated overall package



EXISTING STORM SEWER

PROPOSED STORM SEWER

PROPOSED STORM INLET (CURB & AREA)

PROPOSED MAJOR CONTOUR

PROPOSED MINOR CONTOUR

EXISTING MAJOR CONTOUR

EXISTING MINOR CONTOUR

PROPOSED SWALE

PROPOSED CURB & GUTTER


PROPERTY BOUNDARY

PROPOSED LOT LINE


EXISTING RIGHT OF WAY


FLOOD HAZARD LIMITS


0.00' 10.00' 20.00' 30.00' 40.00' 50.00' 60.00' 70.00' 80.00' 90.00' 100.00'

PROPOSED SPOT ELEVATION 

EXISTING SPOT ELEVATION 00.10 X

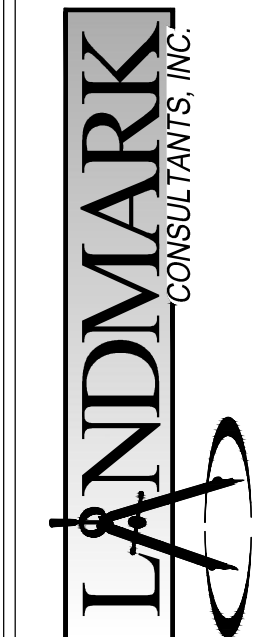
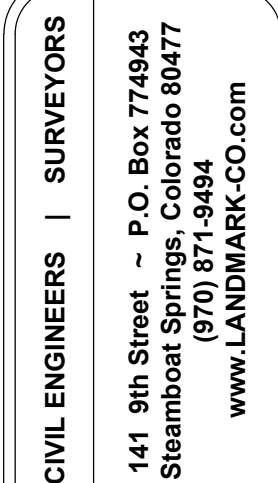
PROPOSED OVERLAND FLOW DIRECTION W/SLOPE 

PROPOSED CHANNELIZED FLOW DIRECTION W/ SLOPE 

EXISTING CHANNELIZED FLOW DIRECTION 

1. THE SIZE, TYPE AND LOCATION OF ALL KNOWN UNDERGROUND UTILITIES ARE APPROXIMATE WHEN SHOWN ON THESE DRAWINGS. IT SHALL BE THE RESPONSIBILITY OF THE DEVELOPER TO VERIFY THE EXISTENCE OF ALL UNDERGROUND UTILITIES IN THE AREA OF THE WORK, BEFORE COMMENCING NEW CONSTRUCTION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR LOCATING ALL UNDERGROUND UTILITIES AND SHALL BE RESPONSIBLE FOR FOR ALL UNKNOWN UNDERGROUND UTILITIES.
2. ALL PROJECT DATA IS ON VERTICAL DATUM: NAVD 88. SEE COVER SHEET FOR BENCHMARK REFERENCES.
3. ELEVATIONS FOR IMPROVEMENTS THAT ARE CONTROLLED BY ADJACENT EXISTING FACILITIES (SUCH AS PROPOSED CUTTERS ALONG EXISTING ADJACENT) SHALL REFLECT THE DATA ON ACTUAL CONDITIONS. COORDINATE WITH ENGINEER TO ENSURE A CONSISTENT SECTION WITH SMOOTH TRANSITIONS WHERE NECESSARY.
4. SEE SOIL REPORT FOR PAVEMENT, SUBGRADE AND MATERIAL PREPARATION, DESIGN AND RECOMMENDATIONS.
5. ALL CURB SPOTS SHOWN ARE FLOWLINE ELEVATIONS, UNLESS NOTED OTHERWISE. ALL OTHER SPOTS ARE FINISHED GRADE ELEVATIONS.
6. LIMITS OF EXISTING FACILITIES OR MATERIALS SHOWN AS REMAINING MAY VARY SUBSTANTIALLY BASED ON ACTUAL CONDITION, CHUNKING, DAMAGE DURING CONSTRUCTION OR OTHER DISCOVERIES. LIMITS OF WORK SHOULD THEREFORE NOT BE CONSIDERED ABSOLUTE OR STATIC AND ADDITIONAL IMPROVEMENTS SHOULD BE ANTICIPATED.
7. PROVIDE POSITIVE DRAINAGE AWAY FROM STRUCTURES AT ALL LOCATIONS. NOTIFY ENGINEER IMMEDIATELY IF CONDITIONS APPEAR TO PRESENT POTENTIAL DRAINAGE FOR A MINIMUM DISTANCE OF 10-FT FROM FOUNDATION.

(FROM C.002 - NOTE #28): DIMENSIONS FOR LAYOUT AND CONSTRUCTION ARE NOT TO BE SCALED FROM ANY DRAWING. IF PERTINENT DIMENSIONS OR ELEVATIONS ARE NOT SHOWN, CONTACT THE DESIGNER FOR CLARIFICATION, AND ANNOTATE THE PROVIDED DIMENSION ON THE AS-BUILT RECORD DRAWINGS. CONTOURS ARE NOT SUITABLE FOR CONSTRUCTION LAYOUT.



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NO.	DATE:	BY:	DESCRIPTION:
4	07.13.22	Grip	Sewer Service Modification
5	09.14.22	Grip	ASI #2 - Additional Spot Elevations, roof lateral
6	09.27.22	Grip	Additional Info for Bus Stop and Headwall
7	02.09.23	Grip	Rev courtyard, updated overall package

ECT:	2387-04
:	1-26-22
ACT:	Grip
:	erikg@landmark-co.com

**REVIEWED
FOR CODE
COMPLIANCE**

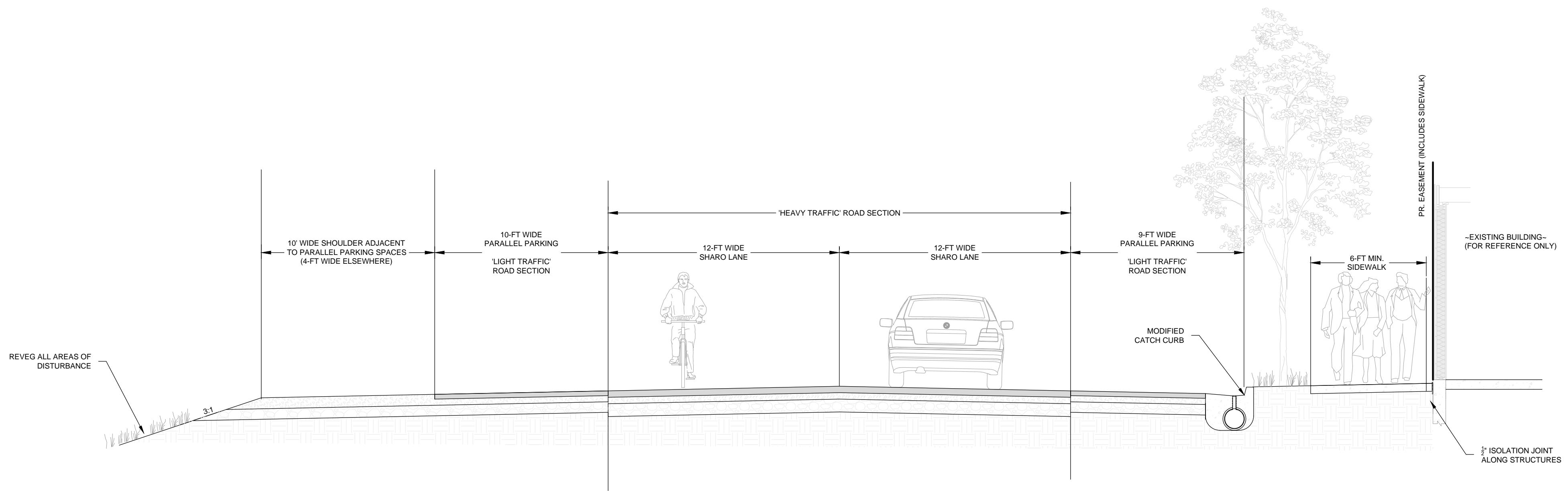
02/24/2023

Steamboat Basecamp - Phase 1

Access Road Plan & Profile

SHEET


C.410



COMMERCIAL PRIVATE ACCESS
(PER SECTION 4.2.5.5 OF THE CITY'S ENGINEERING STANDARDS):

THE INTERNAL ACCESS DRIVE SYSTEM FOR A COMMERCIAL LOT OR DEVELOPMENT AND MAY INCLUDE THE DRIVEWAY, ACCESS DRIVES, AND PARKING AREAS THAT SERVE THE DEVELOPMENT.

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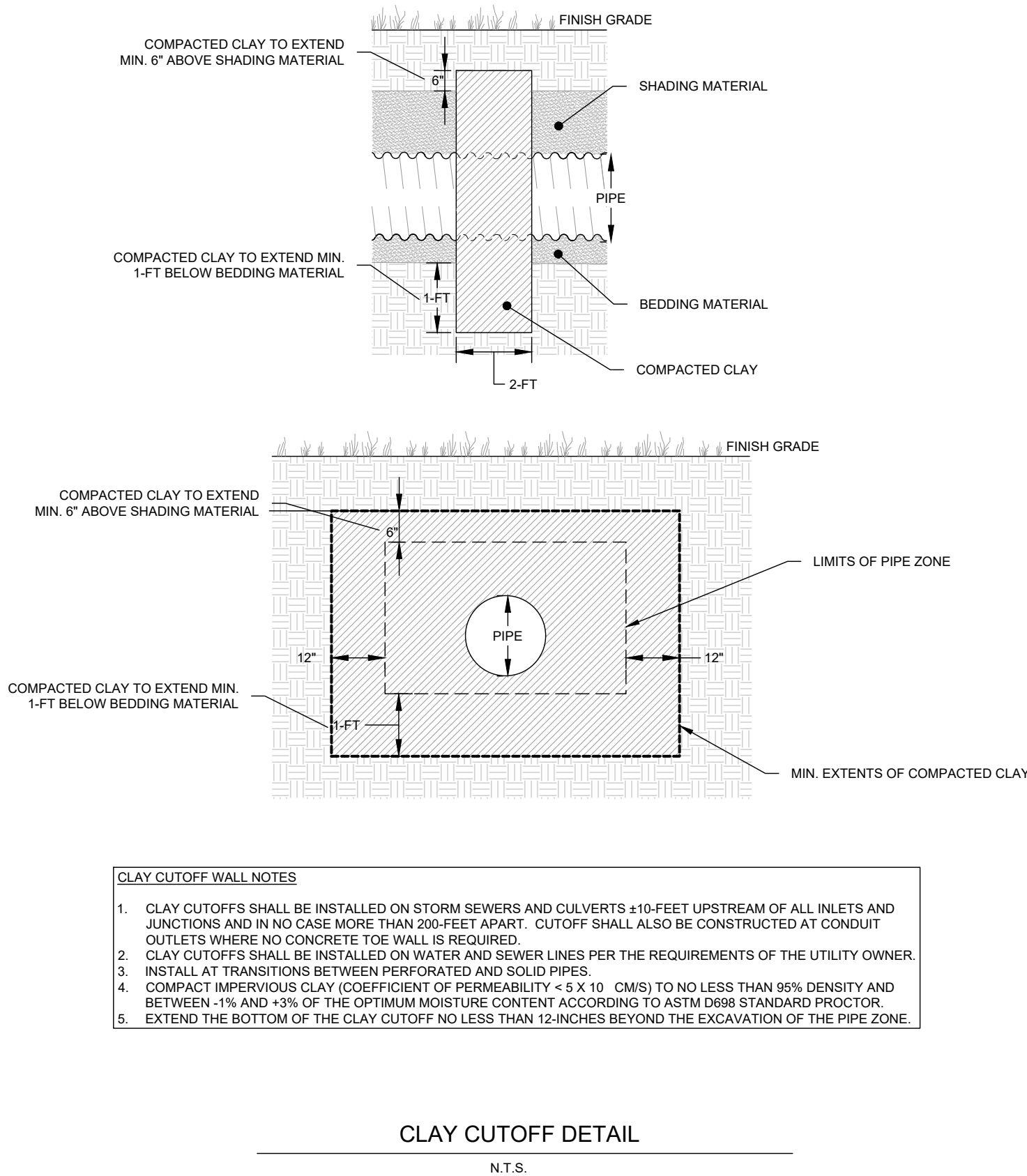


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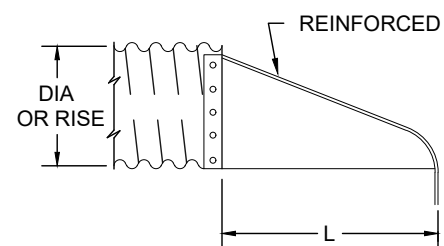
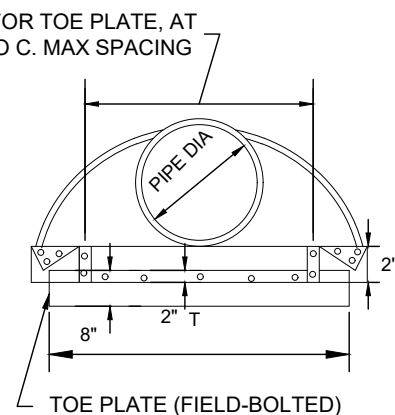
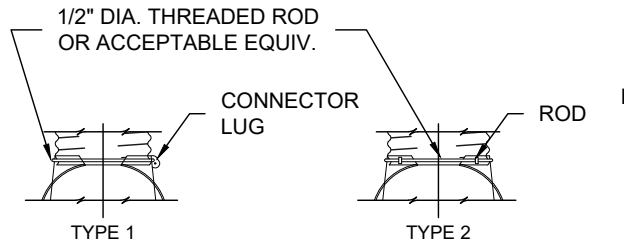
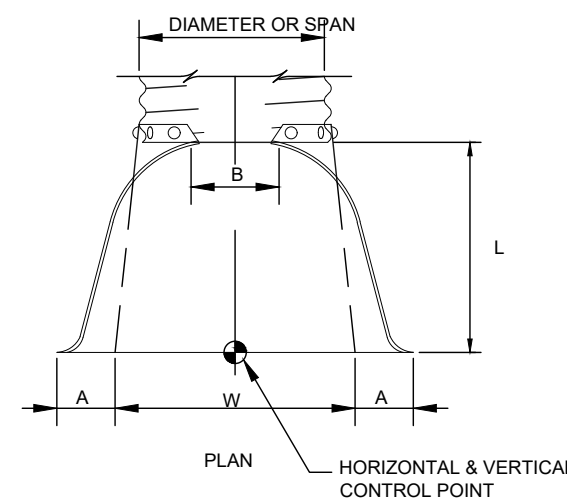
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SIGNATURE AND DATE

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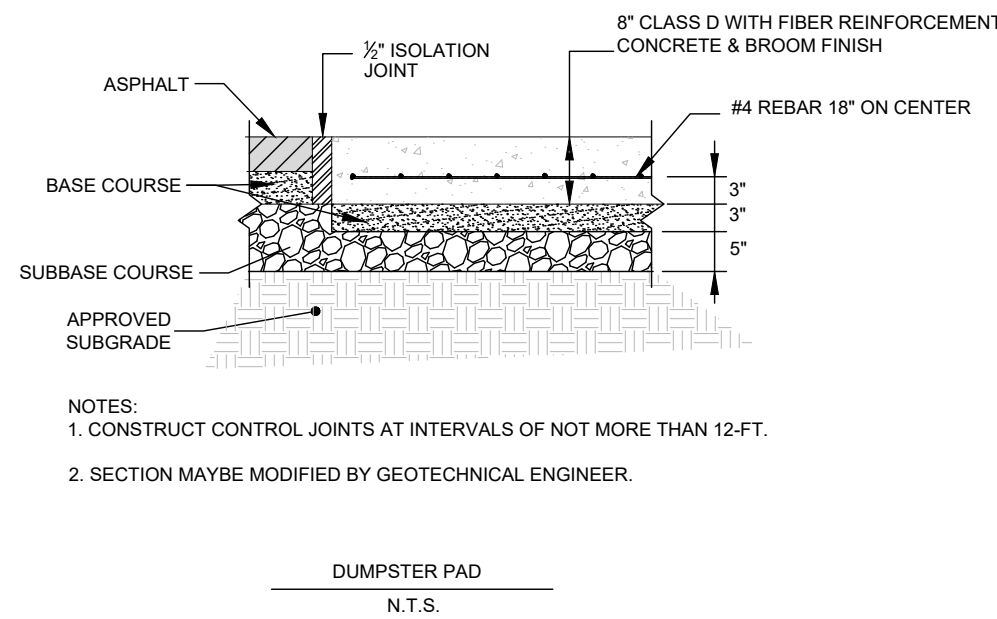
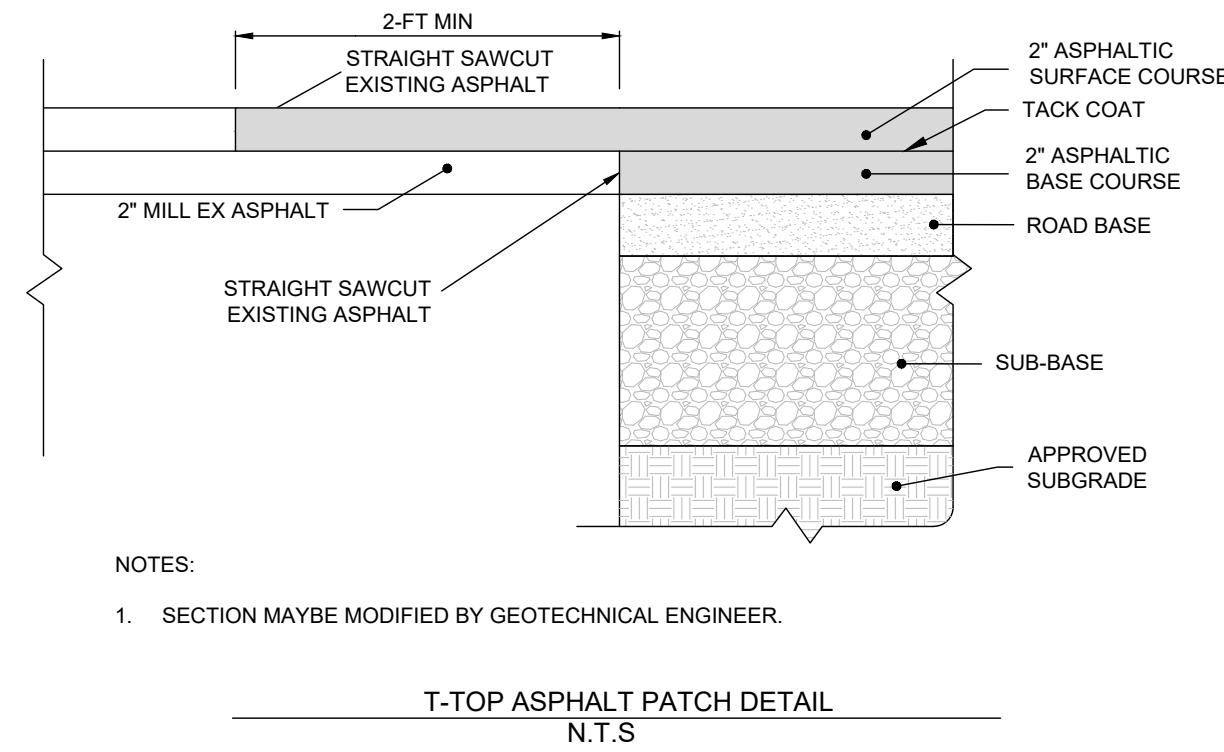
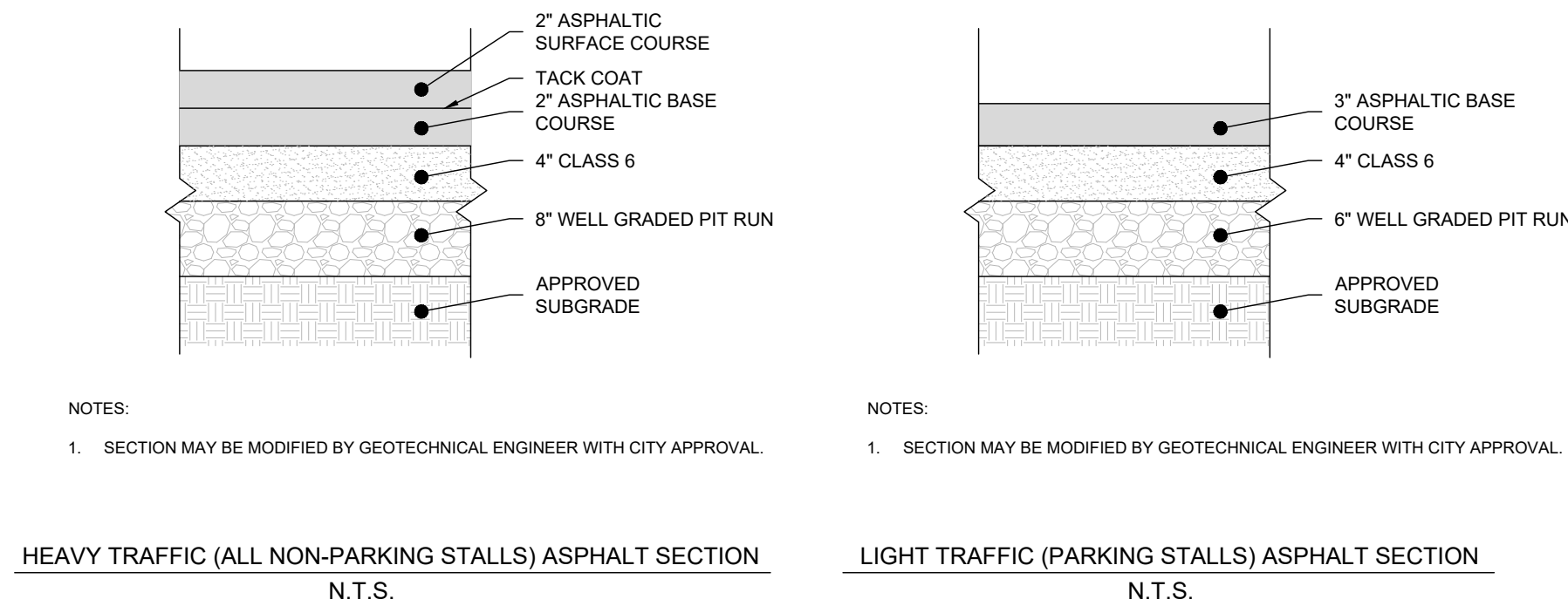


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- Diagram illustrating the Storm Pipe Trench Detail. The trench is shown with a storm pipe (AS-10 to M-45) and a tracer wire. The trench is filled with backfill material. Dimensions are indicated: 8" MIN. BACKFILL TO FINISH GRADE, 4" MIN. RISE, and 15" OR LESS SPAN. The trench is labeled with CLEARANCE, SPAN, and SIDE. The diagram also includes a table for Pipe Span, Bedding Depth, and Max. Side Clearance.
- | PIPE SPAN | BEDDING DEPTH, D | MAX. SIDE CLEARANCE |
|-------------|------------------|---------------------|
| 15" OR LESS | 3" | SPAN |
| 16" TO 30" | 4" | 18" |
| 30" TO 60" | 6" | SPAN/2 |
- PER A.D.S. REQUIREMENTS:
AS-10 TO M-45 (R5 AND R56) OR AS-
APPROVED BY CITY.
- STORM PIPE TRENCH DETAIL
- N.T.S.

- | THIN-WALL ROUND PIPE | | | | | | | |
|----------------------|-------|------------|----|----|----|-----|-----|
| PIPE DIA. | THICK | DIMENSIONS | | | | | |
| | | A | B | H | L | W | T |
| (inches) | | | | | | | |
| 12 | 0.064 | 6 | 8 | 6 | 21 | 24 | 34 |
| 18 | 0.064 | 8 | 10 | 6 | 31 | 36 | 46 |
| 24 | 0.064 | 12 | 16 | 6 | 36 | 42 | 52 |
| 30 | 0.064 | 13 | 16 | 6 | 41 | 48 | 58 |
| 36 | 0.079 | 12 | 16 | 8 | 51 | 60 | 70 |
| 42 | 0.079 | 12 | 16 | 9 | 60 | 72 | 84 |
| 48 | 0.109 | 16 | 22 | 11 | 69 | 84 | 106 |
| 54 | 0.109 | 18 | 27 | 12 | 78 | 90 | 112 |
| 60 | 0.109 | 18 | 30 | 12 | 84 | 102 | 126 |
| 66 | 0.109 | 18 | 36 | 12 | 87 | 114 | 136 |
| 72 | 0.109 | 18 | 36 | 12 | 87 | 120 | 142 |
| 78 | 0.109 | 18 | 39 | 12 | 87 | 126 | 148 |
| 84 | 0.109 | 18 | 42 | 12 | 87 | 132 | 154 |
| 90 | 0.109 | 18 | 48 | 12 | 87 | 138 | 160 |



FLARED END SECTION
N.T.S



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Permit Comments & Pricing

2	03.21.22
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PROJECT: 2387-

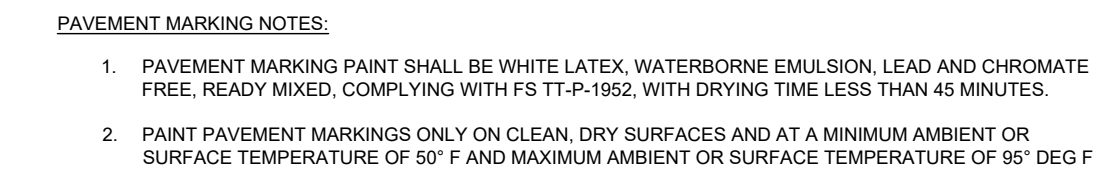
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~~- Phase~~

at Basecam

Steamboat

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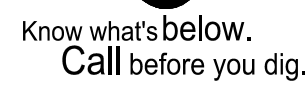


- NOSE TAPER
N.T.S



GUIDE POS
N.T.S

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COLORADO



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UNDERGROUND MEMBER UTILITIES.



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NO.	DATE:	BY:	DESCRIPTION:
2	03.21.22	EG	Permit Comments & Pricing
3	04.27.22	Grip	Permit Comments #2
4	07.13.22	Grip	Sewer Service Modification

PROJECT:	2387-04
DATE:	1-26-22
CONTACT:	Grip
EMAIL:	erikg@landmark-co.com

02/24/2023

Steamboat Basecamp - Phase 1 Details (General)

SHEET

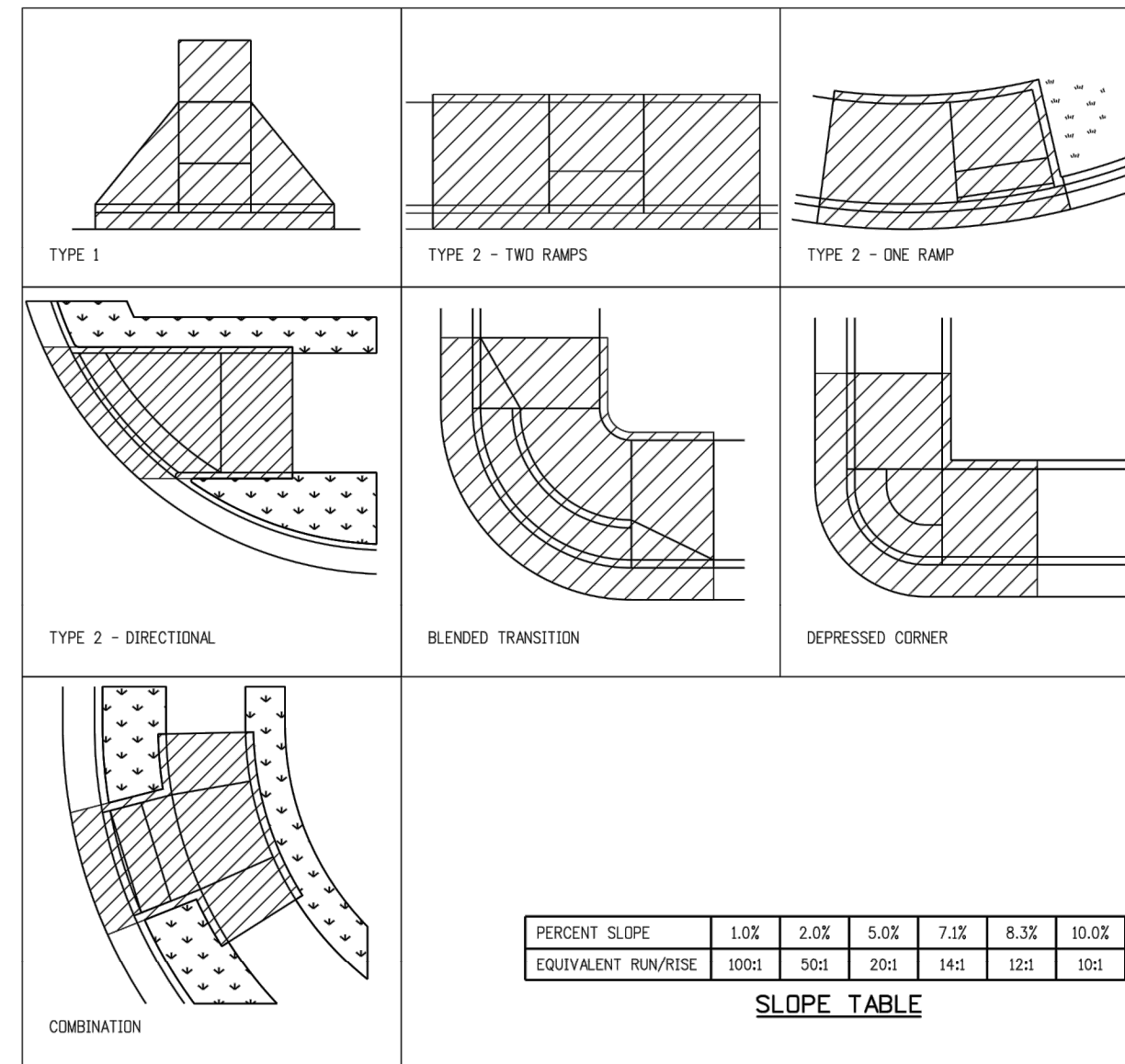
C.501

CURB RAMP GENERAL NOTES:

- IN NEW CONSTRUCTION OR FULL-DEPTH RECONSTRUCTION, PROVIDE A SEPARATE CURB RAMP FOR EACH MARKED OR UNMARKED PEDESTRIAN STREET CROSSING. CURB RAMP SHALL BE CONTAINED WHOLLY WITHIN THE WIDTH OF THE PEDESTRIAN STREET CROSSING OR CROSSWALK THEY SERVE, OR AS SHOWN ON THE CONTRACT PLANS.
- ALTERATIONS ARE DEFINED AS CHANGES TO AN EXISTING HIGHWAY THAT AFFECT PEDESTRIAN ACCESS, CIRCULATION, OR USE. ALTERATIONS INCLUDE, BUT ARE NOT LIMITED TO, RESURFACING, REPAVING, RECONSTRUCTION, CURB RAMP RETROFITS, HISTORIC RESTORATION, OR CHANGES OR REARRANGEMENT TO STRUCTURAL PARTS OR ELEMENTS OF A PEDESTRIAN FACILITY.
- A WALKABLE SURFACE IS DEFINED AS A PAVED SURFACE ADJACENT TO A CURB RAMP OR TURNING SPACE, WITHOUT RAISED OBSTACLES, THAT COULD BE MISTAKENLY TRAVERSED BY A USER WHO IS VISUALLY IMPAIRED.
- IN ALTERATIONS, WHERE AN EXISTING PHYSICAL CONSTRAINT PREVENTS PROVIDING A SEPARATE CURB RAMP FOR EACH PEDESTRIAN STREET CROSSING, A SINGLE DIAGONAL RAMP (ON THE APEX) SHALL BE PERMITTED TO SERVE BOTH PEDESTRIAN STREET CROSSINGS. THE USE OF A SINGLE DIAGONAL RAMP SHALL BE APPROVED BY THE ENGINEER PRIOR TO CONSTRUCTION. DIAGONAL RAMP IS NOT ACCEPTABLE IN NEW CONSTRUCTION OR FULL-DEPTH RECONSTRUCTION.
- DETECTABLE WARNING SURFACES (DWS) ARE INTENDED TO INDICATE THE BOUNDARY BETWEEN A PEDESTRIAN ROUTE AND VEHICULAR ROUTE WHERE THERE IS A FLUSH RATHER THAN CURVED CONNECTION. DWS ARE NOT INTENDED TO PROVIDE WARNING. DWS SHALL BE PROVIDED AT THE FOLLOWING LOCATIONS:
 - CURB RAMP, BLENDED TRANSITIONS, AND DERESSED CORNERS AT PEDESTRIAN STREET CROSSINGS;
 - PEDESTRIAN REFUGE ISLANDS 16 FEET IN WIDTH OR GREATER;
 - BOARDING PLATFORMS AT TRANSIT STOPS WHERE THE EDGE OF THE PLATFORM IS NOT PROTECTED TO PEDESTRIAN CROSS TRAFFIC;
 - BOARDING AREAS AT SIDEWALK OR STREET LEVEL TRANSIT STOPS WHERE THE AREA IS NOT PROTECTED TO PEDESTRIAN CROSS TRAFFIC.
- DETECTABLE WARNING SURFACES SHALL CONTRAST VISUALLY WITH THE ADJACENT GUTTER, HIGHWAY, OR PEDESTRIAN ACCESS ROUTE SURFACE, EITHER LIGHT-ON-DARK OR DARK-ON-LIGHT. FEDERAL YELLOW COLOR IS PREFERRED, HOWEVER, OTHER COLORS MAY BE USED IF APPROVED BY THE ENGINEER.
- IN ALTERATIONS, TO AVOID CHASING GRADE INDEFINITELY ON STEEP ROADWAYS, A CURB RAMP LENGTH IS NOT REQUIRED TO EXCEED 15 FEET REGARDLESS OF THE RESULTING RAMP RUNNING SLOPE.
- ALL SLOPES ARE MEASURED WITH RESPECT TO A LEVEL PLANE.
- DRAINAGE STRUCTURES, TRAFFIC SIGNAL EQUIPMENT, OR OTHER OBSTRUCTIONS SHALL NOT BE INSTALLED ON THE CURB RAMP, OR TURNING SPACE AREAS.
- IN NEW CONSTRUCTION, PULL BOXES, METER BOXES, MAINTENANCE HOLE COVERS, VAULT LIDS, OR SIMILAR, SHALL NOT BE CONSTRUCTED WITHIN ANY PART OF CURB RAMP OR TURNING SPACE. IN ALTERATIONS, WHERE THESE ITEMS CANNOT BE RELOCATED OUTSIDE OF THE CURB RAMP OR TURNING SPACE, THEY MUST NOT CREATE A VERTICAL DISCONTINUITY GREATER THAN 1/2 INCH. ANY VERTICAL DISCONTINUITY BETWEEN 1/4 INCH AND 1/2 INCH SHALL BE BEVELED WITH A SLOPE NOT STEEPER THAN 1:20. THE BEVEL SHALL BE APPLIED ACROSS THE ENTIRE SURFACE DISCONTINUITY.
- CONSTRUCTION OF ANY REQUIRED PEDESTRIAN CURB SHALL BE INCLUDED IN THE BID PRICE OF THE CONCRETE CURB RAMP AND WILL NOT BE PAID FOR SEPARATELY.
- ALL CURB RAMP JOINTS AND GRADE BREAKS SHALL BE FLUSH (0°/10°). THE JOINT BETWEEN THE ROADWAY SURFACE AND THE GUTTER PAN SHALL BE FLUSH.
- THE CONTRACTOR SHALL VERIFY REMOVAL LIMITS ARE SUFFICIENT TO PROVIDE POSITIVE DRAINAGE, MAINTAIN EXISTING DRAINAGE PATTERNS, AND AVOID PONDING IN THE FINAL CONFIGURATION.
- FLARED SIDE SLOPES MAY EXCEED 10.0% ONLY WHERE THEY ABUT A NON-WALKABLE SURFACE, OR WHERE THE ADJACENT RAMP SURFACE IS BLOODED TO PEDESTRIAN TRAFFIC.
- THE CHANGE IN GRADE AT THE BOTTOM OF THE CURB RAMP SHALL NOT EXCEED AN ALGEBRAIC DIFFERENCE OF 13.3% THE COUNTER SLOPE OF THE GUTTER AT THE FOOT OF A RAMP. TURNING SPACE, OR BLENDED TRANSITION SHALL NOT EXCEED 5.0%.
- GRADE BREAKS AT THE TOP AND BOTTOM OF RAMP RUNS SHALL BE PERPENDICULAR TO THE DIRECTION OF THE RAMP RUN. GRADE BREAKS SHALL NOT BE PERMITTED ON THE SURFACE OF THE RAMP RUN OR TURNING SPACE. SURFACE SLOPES THAT MEET AT GRADE BREAKS SHALL BE FLUSH.
- A BROAD FINISH, WITH SWEEPS PERPENDICULAR TO THE DIRECTION OF PEDESTRIAN TRAFFIC, SHALL BE APPLIED TO ALL RAMP AND TURNING SPACE SURFACES.
- IN ALTERATIONS, WHERE A RAMP OR TURNING SPACE MUST TIE INTO AN EXISTING GRADE THAT CANNOT BE ALTERED, THE RAMP OR TURNING SPACE MAY BE WARPED TO TRANSITION TO THE REQUIRED CROSS SLOPE. THE TRANSITION TO THE REQUIRED CROSS SLOPE SHALL BE SPREAD EVENLY OVER THE LENGTH OF THE RAMP OR TURNING SPACE TO MINIMIZE THE DEGREE OF WARPING. THE RATE OF CHANGE ON A RAMP OR TURNING SPACE SHALL NOT EXCEED 3% PER LINEAR FOOT.
- DESIGN AND CONSTRUCT CURB RAMP, TURNING SPACES, AND FLARE SLOPES WITH THE FLATTEST SLOPES POSSIBLE. THE SLOPES INDICATED IN THESE DETAILS SHOW THE MAXIMUM SLOPES ALLOWABLE. PREFERRED VALUES TO BE USED DURING DESIGN, LAYOUT, AND CONSTRUCTION ARE:
 - RAMP RUNNING SLOPE 7.3%
 - TURNING SPACE RUNNING SLOPE 1.5%
 - TURNING SPACE CROSS SLOPE 1.5%
 - FLARE SLOPE 8.0-9.0%

GENERAL NOTES & PAY AREAS

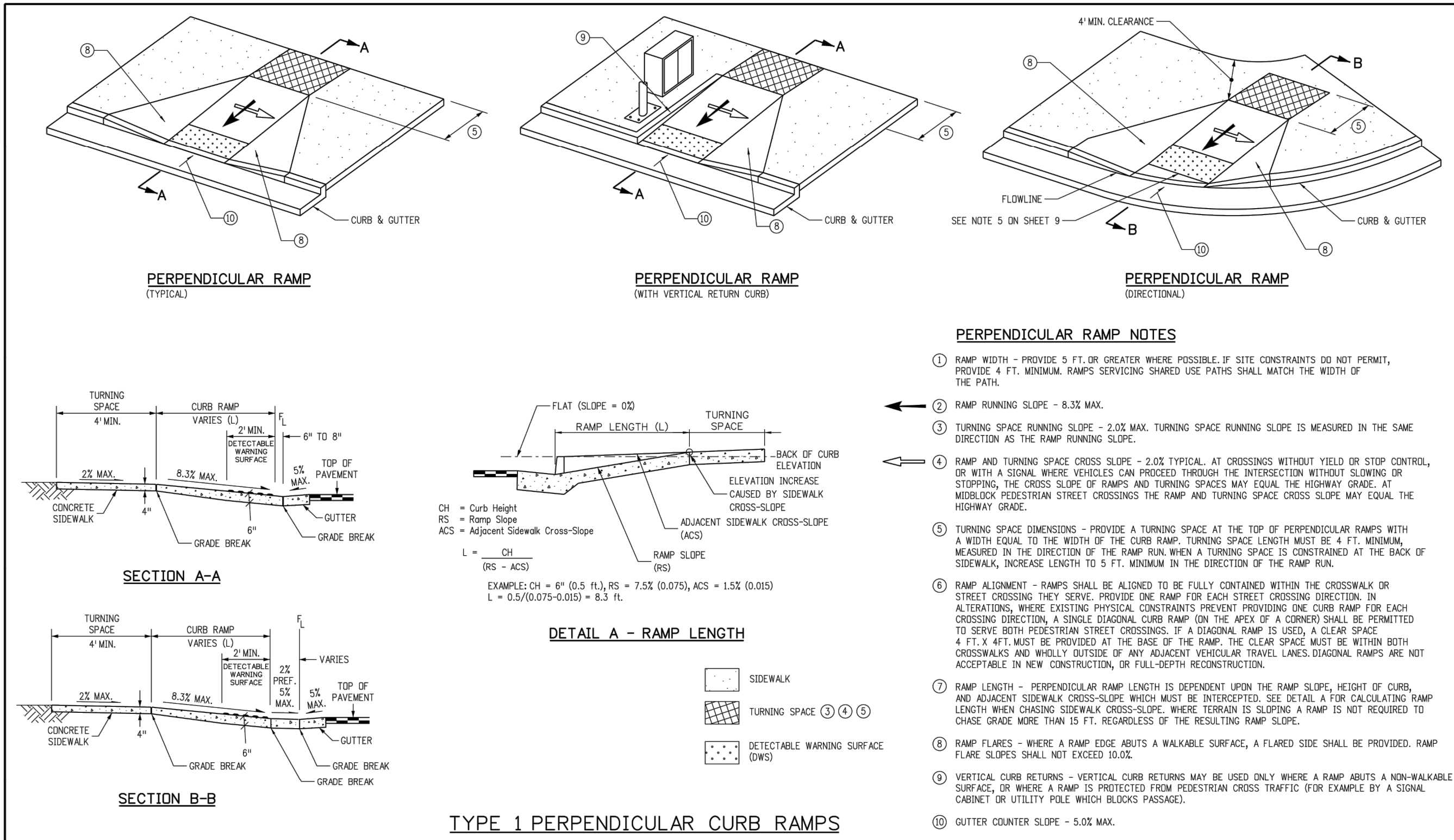
- WHERE SNOW REMOVAL EQUIPMENT WILL BE USED TO CLEAN THE PEDESTRIAN ACCESS ROUTE, CONSULT THE ENGINEER PRIOR TO CONSTRUCTION TO ENSURE THE WIDTH AND THICKNESS OF CURB RAMP IS SUFFICIENT TO ACCOMMODATE SUCH EQUIPMENT.
- PROVIDE EXPANSION JOINT MATERIAL 1/2" THICK WHERE CURB RAMP ADJUGN ANY RIGID PAVEMENT, OR STRUCTURE. THE TOP OF THE JOINT FILLER MATERIAL SHALL BE FLUSH WITH ADJOINING CONCRETE SURFACES. THE EXPANSION JOINT MATERIAL SHALL EXTEND FOR THE FULL DEPTH OF THE CONCRETE SURFACE.
- PROVIDE TIE BAR REINFORCING BETWEEN INDEPENDENTLY POURED CONCRETE CURB RAMP OR TURNING SPACES AND CURB AND GUTTER. DRILL AND GROUT NO. 4 12 INCH LONG REINFORCEMENT BARS (EPOXY COATED) AT 18 INCHES CENTER TO CENTER MINIMUM.

CURB RAMP PAY AREAS**CURB RAMP**

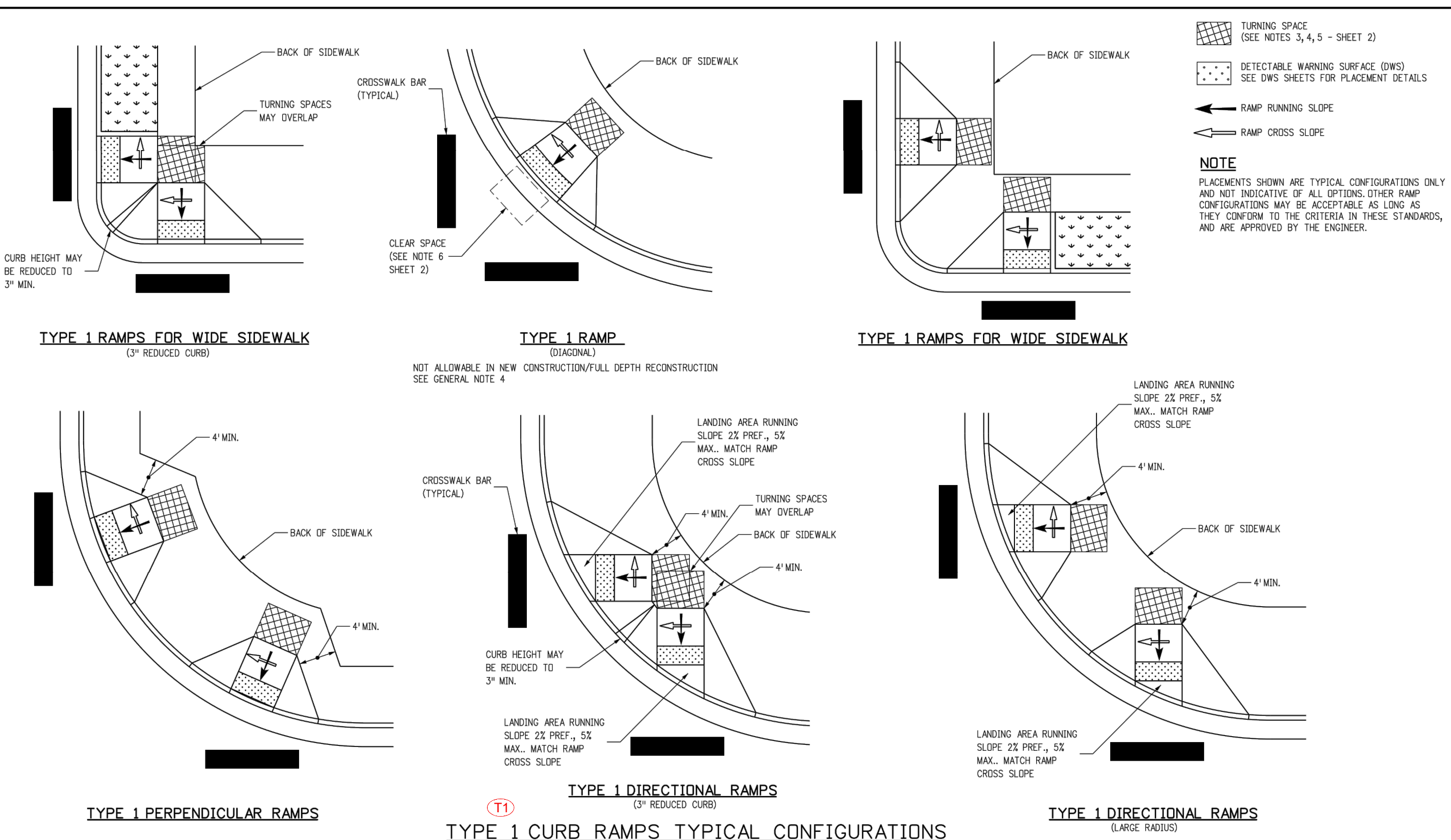
STANDARD PLAN NO.

M-608-1

Sheet No. 1 of 10

**TYPE 1 PERPENDICULAR CURB RAMP**

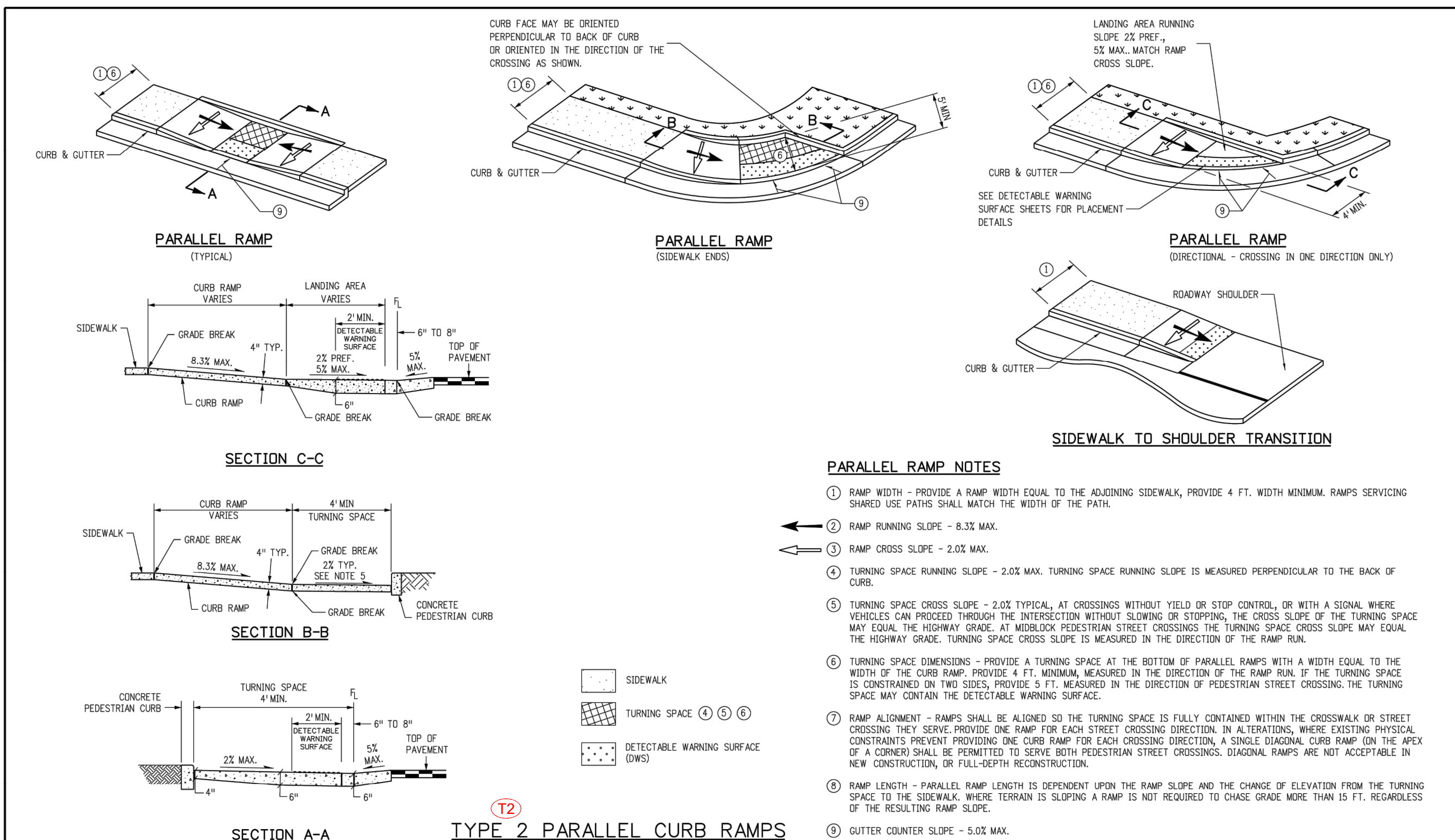
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Last Modification Date: 05/03/19	Initials: LTA	05/03/19	Completely revised every sheet.	Denver, CO 80204	Phone: 303-757-9021 FAX: 303-757-9868				
Full Path: www.codot.gov/business/designsupport				Division of Project Support	JBK/LTA				
Drawing File Name: 6080104010.dgn									
CAD Ver: MicroStation V8	Scale: Not to Scale	Units: English							

**CURB RAMP**

STANDARD PLAN NO.

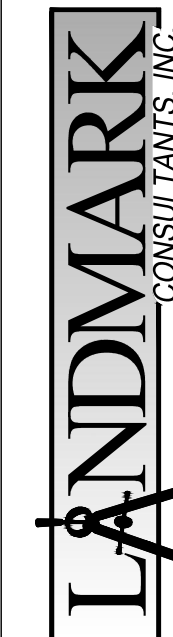
M-608-1

Sheet No. 3 of 10

**TYPE 2 PARALLEL CURB RAMP**

Computer File Information		Sheet Revisions		Colorado Department of Transportation		CURB RAMP		STANDARD PLAN NO.	
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Steamboat Basecamp - Phase

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